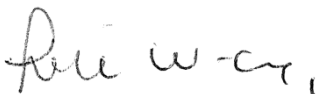


Date of issue: Tuesday, 5 October 2021

MEETING	PLANNING COMMITTEE (Councillors Ajaib (Chair), A Cheema (Vice-Chair), Dar, J Davis, R Davis, Gahir, Mann, Muvvala and S Parmar)
DATE AND TIME:	WEDNESDAY, 13TH OCTOBER, 2021 AT 6.30 PM
VENUE:	COUNCIL CHAMBER - OBSERVATORY HOUSE, 25 WINDSOR ROAD, SL1 2EL
DEMOCRATIC SERVICES OFFICER: (for all enquiries)	NICHOLAS PONTONE 07749 709 868

NOTICE OF MEETING

You are requested to attend the above Meeting at the time and date indicated to deal with the business set out in the following agenda.



JOSIE WRAGG
Chief Executive

AGENDA

PART 1

AGENDA
ITEM

REPORT TITLE

PAGE

WARD

APOLOGIES FOR ABSENCE

CONSTITUTIONAL MATTERS

1. Declarations of Interest

-

-

All Members who believe they have a Disclosable Pecuniary or other Interest in any matter to be considered at the meeting must declare that interest and, having regard to the circumstances described in Section 9 and Appendix B of the Councillors' Code of Conduct, leave the meeting while the matter is discussed.

<u>AGENDA ITEM</u>	<u>REPORT TITLE</u>	<u>PAGE</u>	<u>WARD</u>
2.	Guidance on Predetermination/Predisposition - To Note	1 - 2	-
3.	Minutes of the Last Meeting held on 15th September 2021	3 - 8	-
4.	Human Rights Act Statement - To Note	9 - 10	-
PLANNING APPLICATIONS			
5.	P/16947/002 - 45, The Myrke, Datchet, Slough, SL3 9AB <i>Officer's Recommendation: Approve, subject to conditions.</i>	11 - 38	Upton
6.	P/01303/018 - 79-83, Uxbridge Road, Slough, SL1 1SG <i>Officer's Recommendation: Approve, subject to conditions</i>	39 - 56	Central
7.	P/02683/015 - 204-206, High Street, Slough, SL1 1JS <i>Officer's Recommendation: Delegate to the Planning Manager for Approval</i>	57 - 102	Central
8.	P/00908/012 - 361, Bath Road, Slough, SL1 5QA <i>Officer's Recommendation: Delegate to the Planning Manager for Approval</i>	103 - 172	Cippenham Green
PRE-APPLICATION PRESENTATION			
9.	Queensmere Shopping Centre (including Dukes House and Wellington House), 141, 143, 145 and 165 High Street, and associated land, Slough, SL1 1LN	173 - 182	Central
MATTERS FOR INFORMATION			
10.	Members Attendance Record	183 - 184	-
11.	Date of Next Meeting - 10th November 2021	-	-

Press and Public

Attendance and accessibility: You are welcome to attend this meeting which is open to the press and public, as an observer. You will however be asked to leave before any items in the Part II agenda are considered. For those hard of hearing an Induction Loop System is available in the Council Chamber.

Webcasting and recording: The public part of the meeting will be filmed by the Council for live and/or subsequent broadcast on the Council's website. The footage will remain on our website for 12 months. A copy of the recording will also be retained in accordance with the Council's data retention policy. By entering the meeting room and using the public seating area, you are consenting to being filmed and to the possible use of those images and sound recordings.

In addition, the law allows members of the public to take photographs, film, audio-record or tweet the proceedings at public meetings. Anyone proposing to do so is requested to advise the Democratic Services Officer before the start of the meeting. Filming or recording must be overt and persons filming should not move around the meeting room whilst filming nor should they obstruct proceedings or the public from viewing the meeting. The use of flash photography, additional lighting or any non hand held devices, including tripods, will not be allowed unless this has been discussed with the Democratic Services Officer.

Emergency procedures: The fire alarm is a continuous siren. If the alarm sounds Immediately vacate the premises by the nearest available exit at either the front or rear of the Chamber and proceed to the assembly point: The pavement of the service road outside of Westminster House, 31 Windsor Road.

Covid-19: To accommodate social distancing there is significantly restricted capacity of the Council Chamber and places for the public are very limited. We would encourage those wishing to observe the meeting to view the live stream. Any members of the public who do wish to attend in person should contact the Democratic Services Officer.

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PREDETERMINATION/PREDISPOSITION - GUIDANCE

The Council often has to make controversial decisions that affect people adversely and this can place individual members in a difficult position. They are expected to represent the interests of their constituents and political party and have strong views but it is also a well established legal principle that members who make these decisions must not be biased nor must they have pre-determined the outcome of the decision. This is especially so in “quasi judicial” decisions in planning and licensing committees. This Note seeks to provide guidance on what is legally permissible and when members may participate in decisions. It should be read alongside the Code of Conduct.

Predisposition

Predisposition is lawful. Members may have strong views on a proposed decision, and may have expressed those views in public, and still participate in a decision. This will include political views and manifesto commitments. The key issue is that the member ensures that their predisposition does not prevent them from consideration of all the other factors that are relevant to a decision, such as committee reports, supporting documents and the views of objectors. In other words, the member retains an “open mind”.

Section 25 of the Localism Act 2011 confirms this position by providing that a decision will not be unlawful because of an allegation of bias or pre-determination “just because” a member has done anything that would indicate what view they may take in relation to a matter relevant to a decision. However, if a member has done something more than indicate a view on a decision, this may be unlawful bias or predetermination so it is important that advice is sought where this may be the case.

Pre-determination / Bias

Pre-determination and bias are unlawful and can make a decision unlawful. Predetermination means having a “closed mind”. In other words, a member has made his/her mind up on a decision before considering or hearing all the relevant evidence. Bias can also arise from a member’s relationships or interests, as well as their state of mind. The Code of Conduct’s requirement to declare interests and withdraw from meetings prevents most obvious forms of bias, e.g. not deciding your own planning application. However, members may also consider that a “non-pecuniary interest” under the Code also gives rise to a risk of what is called apparent bias. The legal test is: “whether the fair-minded and informed observer, having considered the facts, would conclude that there was a real possibility that the Committee was biased’. A fair minded observer takes an objective and balanced view of the situation but Members who think that they have a relationship or interest that may raise a possibility of bias, should seek advice.

This is a complex area and this note should be read as general guidance only. Members who need advice on individual decisions, should contact the Monitoring Officer.

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Planning Committee – Meeting held on Wednesday, 15th September, 2021.

Present:- Councillors Ajaib (Chair), A Cheema (Vice-Chair), Dar, R Davis, Gahir, Mann, Muvvala and S Parmar

Also present under Rule 30:- Councillors Basra, Grewal, Hulme and Swindlehurst

Apologies for Absence:- Councillor J Davis

PART I

41. Declarations of Interest

Item 5 (Minute 48 refers) – Akzo Nobel site, Wexham Road: Councillor Mann declared that she had met the applicant prior to submission of the application. She stated she had an open mind in relation to the application in front of the committee.

Item 5 (Minute 48 refers) – Akzo Nobel, Wexham Road: Councillors Dar and Gahir declared that their ward (Wexham Lea) adjoined the application site and was therefore impacted by development of the site. They stated they would approach the applications with open minds.

Item 7 (Minute 49 refers) – 2A Bower Way: All members of the Committee had received emails from the applicant and interested parties, but would all approach the applicant with open minds.

Items 7 (Minute 49 refers) - 2A Bower Way and 10 (Minute 51) – 324-374 Bath Road: Councillor R Davis declared the sites were in his ward. In relation to the application for Bower Way Councillor Davis stated that he would approach it with an open mind. In relation to the pre-application for 324-374 Bath Road it was noted that the incorrect ward was stated in the report and the site was in fact in Haymill & Lynch Hill, not Councillor Davis' ward of Cippenham Green as stated.

Item 8 (Minute 47) – The Myrke: Councillor Ajaib declared that he had been contacted by interested parties in relation to the application but would approach it with an open mind.

Item 9 (Minute 50 refers) – Riverside Park, Poyle: Councillor Cheema declared that the application was in her ward and that she would approach it with an open mind.

42. Guidance on Predetermination/Predisposition - To Note

Members confirmed that they had read and understood the guidance on predetermination and predisposition.

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43. Minutes of the Last Meeting held on 28th July 2021

Resolved – That the minutes of the meeting held on 28th July 2021 be approved as a correct record.

44. Human Rights Act Statement - To Note

The Human Rights Act statement was noted.

45. Planning Applications

The Amendment Sheet, which included details of alterations and amendments received since the agenda was circulated had been sent to Committee Members and published on the Council website. Members confirmed they had received and read it prior to the consideration of planning applications.

Oral representations were made to the Committee under the Public Participation Scheme prior to the applications being considered by the Committee as follows:-

Application P/00072/108 – AkzoNobel Site, Wexham Road – the applicant and Ward Member, Councillor Hulme, addressed the Committee.

Application P/16742/002 – 31-41 Beckwell Road, Slough – an Objector addressed the Committee.

Application P/01125/009 – 2A Bower Way, Slough – the agent and Ward Member, Councillor Swindlehurst, addressed the Committee.

Application P16947/002 – 45 The Myrke, Datchet – two Objectors, the applicant and Ward Members, Councillors Basra and Grewal, addressed the Committee.

Resolved – That the decisions taken in respect of the planning applications as set out in the minutes below, subject to the information, including conditions and informatives set out in the report of the Planning Manager and the Amendment Sheet circulated to Members prior to the meeting and subject to any further amendments and conditions agreed by the Committee.

46. P/16742/002 - 31-41 Beckwell Road, Slough, SL1 2XA

Application	Decision
Demolition of existing properties and proposed redevelopment of the site to allow for a part 3 and part 4 storey building to form 29 self contained residential units - 16 x 1 Bed Units and 13 x 2 Bed Units with associated	Delegated to the Planning Manager for refusal for the reasons set out in paragraph 1.1 of the Planning Officers report.

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amenity space, refuse and recycling, cycle storage and basement car park.	
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47. P/16947/002 - 45 The Myrke, Datchet, Slough, SL3 9AB

Application	Decision
Construction of 2 no. semi-detached dwellings and 1 no. detached dwelling.	Deferred for a site visit to enable to committee to observe the width of access to the site to service properties.

48. P/00072/108 - Akzo Nobel Site, Wexham Road, Slough, SL2 5DS

Application	Decision
Approval of reserved matters following the outline approval reference P/00072/096 dated 19th November 2020 for the mixed use development of land at the former Akzonobel Decorative Paints facility, Wexham Road, Slough SL2 5DB. Reserved matters application for full details of access (internal site arrangements), appearance, layout, scale, and landscaping for the first phase of the approved commercial floorspace, comprising data centre use (including ancillary office space and associated plant and infrastructure provision); car parking, landscaping and vehicular and pedestrian access.	<p>Delegated to the Planning Manager for approval, subject to finalising conditions.</p> <p>It was agreed that an informative would be issued to remind the applicant of the existing conditions, particularly relating to the Construction Management Plan and restrictions on HGV movements.</p>

During discussion of the item the Committee discussed the s106 agreement relating to the outline application. It was agreed that the Planning Manager would circulate further information to the committee in relation to the agreed s106 contributions, particularly for tree planting and play space.

49. P/01125/009 - 2A Bower Way, Slough, SL1 5HX

Application	Decision
Demolition of existing building and construction of 11 no flats. 6 x 2 bedroom flats and 5 x 1 bedroom flats	Refused, for the reasons set out in paragraph 21.1 of the Planning Officers report.

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with 11 car parking spaces/15 cycle storage spaces and amenity space at the rear.	
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50. P/19460/000 - Riverside Park, Millbrook Way, Slough, Poyle, Slough, SL3 0HG

Application	Decision
Demolition of all existing buildings and redevelopment of the site, including change of use from office use, for flexible light industrial, general industrial, and storage and distribution employment floor space within a single building, with associated service yards, car parking and landscape planting.	Delegated to the Planning Manager for approval, in order to finalise conditions and agree pre-commencement conditions; and any other minor changes.

51. 324-374 Bath Road, Cippenham

The Committee received a pre-application presentation on the proposals for 324-374 Bath Road, Slough.

Members were reminded of the purpose, scope and format of the discussion, as outlined in the Code of Conduct for Councillors and Officers in relation to planning and licensing matters.

The pre-application presentation was given by representatives of the applicant. The proposal was to demolish all existing buildings and structures and conduct a phased construction of a residential development (Use Class C3) comprising a series of buildings of between 6-11 storeys, and all associated infrastructure, access, plant, public and semi-private realm, car/cycle parking, and hard and soft landscaping. 319 apartments and retail space were projected.

Members were given the opportunity to ask questions and make initial observations on the proposal which are summarised as follows:

- The housing mix and provision of affordable housing were important issue for the Committee. It was noted that the provisional mix was 50% studio and 1-bed; 45% 2-bed; and 5% 3-bed.
- Members commented on the opportunities to maximise family sized properties on the site.
- Work was ongoing regarding affordable provision, including discussions with the Council’s housing officers.
- Several Members commented on whether the proposed heights of up to 11 storeys would be in keeping with the surrounding area.

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- Although the site was accessible, adequate car parking would be required to avoid overspill from the site.
- The concept designs and public realm improvements were received positively and the principle of development to provide new housing and improve the area was welcomed.

At the conclusion of the discussion, the presentation was noted.

Resolved – That the pre-application presentation be noted.

52. Slough Local Plan - Proposed consultation on the release of Green Belt sites for family housing

The Committee considered a report that sought Member's views on the proposed release of Green Belt sites for housing prior to the Cabinet meeting on 20th September which would decide whether to commence a public consultation.

The Planning Policy Manager summarised the report and highlighted the context that there was a shortage of housing in Slough to meet the needs of our growing young population and that there was a particular need for more family housing. Most sites in Slough were not suited to delivering family housing but it should be viable to provide such housing and affordable housing on greenfield sites. Members noted that around 250 houses could be built on "suitable" sites set out in the report and 350 on "possible" sites.

The proposed consultation was an important part of the Local Plan by demonstrating progress in considering all options to bring forward sites and helping to defend the other elements of the Spatial Strategy such as protecting the suburbs and cross border expansion of Slough. The sites contained in the consultation had been assessed using a range of planning criteria and had been subject to two previous consultations. A 'traffic light' system was used following a preliminary assessment to indicate whether it was considered sites were 'suitable', 'possible' or 'unsuitable'. The Officer reviewed the list of sites based on this initial assessment. The Committee was asked to note and comment on the proposed consultation.

Members recognised the sensitivity of the release of Green Belt land and noted the process that had been outlined. Technical assessments would need to be carried out for each site and they could only be released if there were exceptional reasons. It was confirmed that if any future decision was made to release any of the sites for development it would be for family housing, which could include apartments of a suitable size. A number of questions and comments were made about specific sites during the course of the discussion, including the suitability of land south of Blenheim Road which was adjacent to the Kings Reach housing development released from the Green Belt in the 2004 Local Plan. The Leader of the Council was present to listen to the comments of the Planning Committee and he emphasised that this was the start of a consultation process and it was hoped there would be a high level of public engagement and feedback.

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At the conclusion of the discussion the Committee noted the arrangements for the proposed consultation.

Resolved –

- (a) That the proposed consultation on the release of Green Belt sites for family housing be noted.
- (b) That the comments of the committee on the report and proposed consultation be forwarded to the meeting of the Cabinet on 20th September.

53. Planning Appeal Decisions

Members received and noted details of planning appeals determined since the previous report to the Committee. Members commented on the number of successful appeals, which it was recognised varied over time. It was agreed that future reports would show the percentage of appeals upheld and a breakdown of the number of those where the Committee had voted against an Officer recommendation.

Resolved – That details of planning appeals be noted.

54. Members' Attendance Record

Resolved – That the record of Members' attendance for 2021/22 be noted.

55. Date of Next Meeting - 13th October 2021

The date of the next meeting was confirmed as 13th October 2021.

Chair

(Note: The Meeting opened at 6.35 pm and closed at 10.57 pm)

The Human Rights Act 1998 was brought into force in this country on 2nd October 2000, and it will now, subject to certain expectations, be directly unlawful for a public authority to act in a way which is incompatible with a Convention Right. In particular Article 8 (Respect for Private and Family Life) and Article 1 of Protocol 1 (Peaceful Enjoyment of Property) apply to planning decisions. When a planning decision is to be made, however, there is further provision that a public authority must take into account the public interest. In the vast majority of cases existing planning law has for many years demanded a balancing exercise between private rights and public interest, and therefore much of this authority's decision making will continue to take into account this balance.

The Human Rights Act 1998 will not be referred to in the Officers Report for individual applications beyond this general statement, unless there are exceptional circumstances which demand more careful and sensitive consideration of Human Rights issues.

Please note the Ordnance Survey Maps for each of the planning applications are not to scale and measurements should not be taken from them. They are provided to show the location of the application sites.

CLU / CLUD	Certificate of Lawful Use / Development
GOSE	Government Office for the South East
HPSP	Head of Planning and Strategic Policy
HPPP	Head of Planning Policy & Projects
S106	Section 106 Planning Legal Agreement
SPZ	Simplified Planning Zone
TPO	Tree Preservation Order
LPA	Local Planning Authority

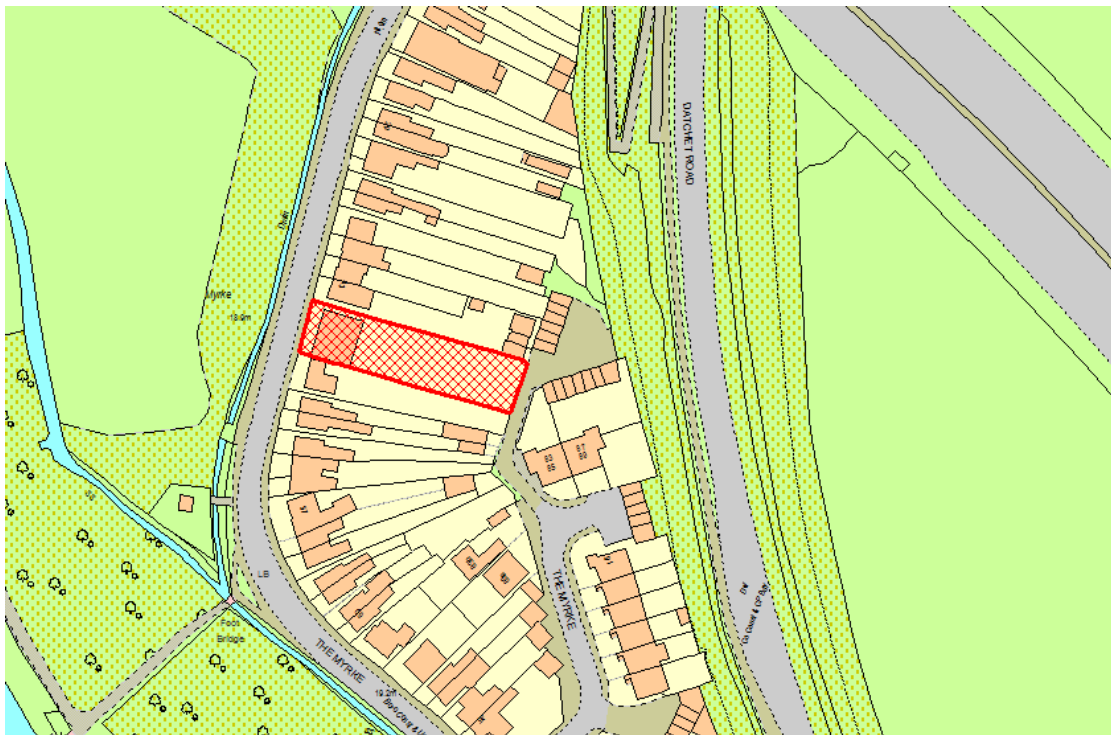
	OLD USE CLASSES – Principal uses
A1	Retail Shop
A2	Financial & Professional Services
A3	Restaurants & Cafes
A4	Drinking Establishments
A5	Hot Food Takeaways
B1 (a)	Offices
B1 (b)	Research & Development
B1 (c)	Light Industrial
B2	General Industrial
B8	Warehouse, Storage & Distribution
C1	Hotel, Guest House
C2	Residential Institutions
C2(a)	Secure Residential Institutions
C3	Dwellinghouse
C4	Houses in Multiple Occupation
D1	Non Residential Institutions
D2	Assembly & Leisure

	OFFICER ABBREVIATIONS
DR	Daniel Ray
ADJ	Alistair de Jeux
PS	Paul Stimpson
NR	Neetal Rajput
HA	Howard Albertini
JG	James Guthrie
SB	Sharon Belcher
IK	Ismat Kausar
CM	Christian Morrone
AH	Alex Harrison
NB	Neil Button
MS	Michael Scott
SS	Shivesh Seedhar
NJ	Nyra John
KP	Komal Patel
WD	William Docherty

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Registration Date:	18 th May 2020	Application No:	P/16947/002
Officer:	Alex Harrison	Ward:	Upton
Applicant:	Alexa Reid	Application Type:	Minor
		8 Week Date:	31 st October 2020
Agent:	Andy Patrick Ian Belsham Associates		
Location:	45, The Myrke, Datchet, Slough, SL3 9AB		
Proposal:	Construction of 2 no. semi-detached dwellings and 1 no. detached dwelling		

Recommendation: Approve, subject to conditions.



P/16947/002 – 45 The Myrke, Datchet, Slough, SL3 9AB

1.0 SUMMARY OF RECOMMENDATION

1.1 Under the current constitution this application is being brought to Committee for decision following a call in request from Cllr Basra. The application was deferred from the Planning Committee meeting of 15 September 2021 to allow for a Members' Site Inspection.

1.2 Having considered the relevant policies set out below, the representations received from all consultees and neighbouring residents, as well as all other relevant material considerations, it is recommended that the application be approved subject to conditions.

PART A: BACKGROUND

2.0 Proposal

2.1 Full planning permission is sought for the construction of 2 no. semi-detached dwellings and 1 no. detached dwelling.

2.2 The proposed development consists of:

- The erection of a pair of semi detached dwellings each with two bedrooms
- The erection of a single detached dwelling with three bedrooms
- Hardstanding and garden space associated with the above

2.3 Please note that although the description of the development remained the same, amended drawings were received from the agent on 28th August 2020 and 10th December 2020. Amended highways drawing were received 12th August 2021.

3.0 Application Site

3.1 The site consists of a plot of land to the rear of numbers 41, 43 and 45 The Myrke. Access is taken from an access road to The Myrke to the east. The site is therefore surrounded by residential development to the north, south and west. The character of the local area is residential, with a mix of two storey detached, semi detached and terraced properties.

4.0 Site History

4.1 Although there is some planning history associated with number 45 The Myrke, there is no planning history specific to the proposed site.

5.0 Neighbour Notification

5.1 69, The Myrke, Datchet, Slough, SL3 9AB, 93, The Myrke, Datchet, Slough, SL3 9AD, 81, The Myrke, Datchet, Slough, SL3 9AB, 71, The Myrke, Datchet, Slough, SL3 9AB, 83a, The Myrke, Datchet, Slough, SL3 9AB, 51, The Myrke, Datchet, Slough, SL3 9AB, 97, The Myrke, Datchet, Slough, SL3 9AD, 99, The Myrke, Datchet, Slough, SL3 9AD, 57-59, The Myrke, Slough Road, Slough, Berkshire, SL3 9AB, 85, The Myrke, Slough, SL3 9AB, 47, The Myrke, Slough, SL3 9AB, 83, The Myrke, Slough, SL3 9AB, 87, The Myrke, Slough, SL3 9AB, 39, The Myrke, Slough, SL3 9AB, 43, The Myrke, Slough, SL3 9AB, 41, The Myrke, Slough, SL3 9AB, 89, The Myrke, Slough, SL3 9AD, 79, The Myrke, Slough, SL3 9AB, 113, The Myrke, Slough, SL3 9AD, 83B, The Myrke, Slough, SL3 9AB

Local residents were consulted on the original scheme on 26th May 2020 with additional consultee addresses added on 16th June 2020. Local residents were then re-consulted on the amended drawings on 14th September 2020.

5.2 At the time of writing there have been eighteen letters of representation although it should be noted that there are sometimes more than one representation from an individual property. In summary the comments received are as follows:

- The private access track inadequate and is not sufficiently wide to deal with the vehicle movements to the new dwellings
- Manoeuvring vehicles on the access track is already difficult and this will be worsened by the proposed development
- Access to existing driveways and garages will be impacted by the proposed scheme
- Concerns regarding the upkeep of the access road and damage to fences
- Concerns regarding proposal and an increased pressure on car parking on The Myrke
- Parking restrictions on local roads may assist with the problem of parking by non residents
- Concerns that large vehicles (including emergency vehicles) will not be able to access the site
- Two car parking spaces per dwelling is unlikely to be adequate. Visitor car parking should be provided.
- The height of the proposed dwellings will result in overlooking towards neighbouring properties
- The height of the proposed semi detached dwellings means that they will be taller than those in the surrounding area
- Concerns about possible change of use of outbuilding to residential accommodation in local area
- The proposal is overdevelopment of the site
- Concerns that the waste storage collection point will result in disruption to local residents
- Concerns regarding potential impact on amenity of numbers 83, 85, 87 and 89 The Myrke
- Concerns regarding the impact on the character of the area
- Concerns regarding the sustainability/energy standards of the proposed

development

- Concerns regarding the potential impact on trees
- Concerns regarding foul drainage
- Concerns regarding potential noise and disturbance during the construction period
- There is no street lighting which would result in increased risk of crime/risk to safety.
- No yellow site notice has been erected.
- Access to the garages opposite the site will be restricted and a covenant will need to be agreed in order to allow rights of access to these garages
- Surface water drainage is inadequate
- Concerns regarding impact on ecology
- Concerns regarding the impact on services (television/broadband)

5.3 A petition containing 27 signatures (although it should be noted that there are occasions when there are multiple signatures from the same address) has been received objecting on the following basis:

1. **LOSS OF PRIVACY** – The proposed development is higher than existing neighbouring properties which will affect the residents privacy by overlooking adjoining properties.
2. **TRAFFIC** – The proposed development will generate extra traffic due to site workers and delivery vehicles in an already extremely congested cul-de-sac. Also no provision has been made for visitors to the new development.
3. **NOISE** – The proposed developments close proximity to other properties will cause excessive noise and disturbance to neighbouring residents.
4. **OVERBEARING NATURE OF DEVELOPMENT** – The proximity of the proposed development will result in significant loss of outlook to neighbouring properties.
5. **SAFETY**- The access road to the proposed development is extremely narrow. It was only meant for access to the garages and is not suitable for large lorries or emergency vehicles. (As demonstrated recently when there was a fire in a neighbouring garden and the fire appliance could not negotiate the narrow lane)

5.4 A letter of objection has been received from Cllr Basra prior to the September Committee which read as follows:

The date of the first issue was back in November 2017 with revisions In 2018 and again in 2020. Councillors and residents have actively been in discussion with SBC with regards to their Objections. With the delays due to COVID – 19, the changes to the Planning Officer and endless months of uncertainty we received the email below informing us that this application was to be on the agenda for the September Planning Committee with a recommendation for approval.

As Councillors, we did not receive confirmation from Democratic Services that this was definitely going ahead or details of the right to speak as I called in the application to Committee as a Ward Member, was this an oversight? The papers were issued on 7th September with a resident only receiving her letter on 10th September. The papers now state that the Officer's Recommendations are for approval, subject to conditions.

As Councillors we met again with our residents on Friday 10th September, a

number of who are away and do not return until after the proposed hearing. They are very stressed and do not have all the answers to the following points:-

- Has a Site Visit been conducted – if so when and what was the findings, can a report be made available?
- There are a number of assumptions in the papers which highlight Health & Safety issues and concerns which have been already been put forward by residents, particularly with regards to the Access Road on pages 137 to 139 of the report.
- Would the road leading to this planning application be adopted by SBC as is the road leading to this application, and what impact will this have on existing residents whose parking spaces are already restricted?
- Residents have lost trust and faith in our council as a confirmation was received on 6th August 2020 with regards to a breach of planning control for a garage which is being used as a dwelling at 49 The Myrke. More than a year on we have been informed that this application is yet to be determined and may be heard in the next few weeks, pictures attached. The resident who lives just feet's away from this dwelling has approached our local MP for his help in this matter. Emergency services cannot access beyond this site which will have an impact on this application.
- Over a year ago Councillors/Residents raised concerns about the garages at the same site which were being sold for potential development, this did not take place then but has suddenly remerged to be auctioned on 24th September together with the land which includes the road leading into the very narrow access road, the access road in front of this Planning application, 6 of the garages and additional land at the rear with the potential of 2 more semi-detached houses being built.

6.0 Consultations

6.1 Transport and Highways

Comments dated 12th July 2020 on the original scheme.

Parking Spaces

- Although parking spaces must dimension 2.4x4.8m, this applies to an open layout without obstructions. For parking spaces sited next to vertical obstructions such as fences, landscaping, hedges, etc the width of the bays must be increased by 300mm on the side where there is an obstruction to allow the doors to be opened and spaces used as intended. The risk of not doing so is that vehicles stop in the road outside of the bays for passengers to embark/disembark both causing an obstruction (which may not be such an issue on this access road) or also pose safety concerns with young children having to get out of the car in the road before it is parked. The layout must be amended to allow for this.
- The parking layout for unit 3 appears to be very tight with the bays almost having a slight overlap. The applicant should provide vehicle tracking to demonstrate that the bay parallel to the road can be used. Tracking should be carried out used a DB32 large car (which measures 5.1m in length).

Visibility

- The applicant has not demonstrated any visibility splays for the parking bays, these must be demonstrated as inadequate visibility poses road

safety hazards to pedestrians and other users of the access road.

Boundary Treatment

- We assume that there is an open boundary along the frontage of the proposed site as the road is not wide enough for vehicles to manoeuvre meaning they would need to use part of the access road and part of their frontage to turn vehicles in/out of the parking bays, especially Unit 1 where the roadway is the narrowest.

Bin Collection Point

- This appears to be designed to accommodate 3no wheelie bins. As there are 3no dwellings being proposed, the bin collection point must be able to accommodate 6no 240l wheelie bins on collection day. This point should only be used on collection day, at all other times the bins should be stored within the curtilage of the individual properties and storage for 3no wheelie bins for each dwelling must be shown (residual waste, recycling, garden waste). As all gardens have rear access, I'm confident that each property can accommodate the bins as required.
- The bin collection point is approximately 25m from a point on the adopted highway where the refuse vehicle would be able to collect the bins therefore this is deemed acceptable.

Shared Surface Access Road

- Access to the properties is proposed from the existing narrow road serving the garage complex.
- It is unclear if the applicant has a right of access over this road for the purposes intended (access for properties and to carry out works on the road) but it is assumed that they have and this is not a Highways matter as such.
- The applicant has proposed the access road as a shared surface however has not provided any details on the width, construction or design of the road. Shared surface roads must be suitably designed in accordance with the relevant guidance and standards to ensure they are fit for purpose and safe to use. DfT guidance requires us to ensure that all schemes are designed with the needs of different users in mind, and satisfy their obligations under the equalities legislation. As such no details have been provided in terms of a shared surface design which needs to accommodate the residents of the new dwellings all users associated with the dwellings as well as the existing users of the road. A suitable design would need to be submitted for approval before it could be supported to ensure the shared surface is both safe and fit for purpose.
- The applicant has made reference to resurfacing the road however, no details of construction/materials have been submitted and considering that this is being considered as a shared surface road, the construction of the road must be suited to this use in accordance with guidelines.
- Shared surface roads must have entry/exit points also known as gateways. Details of this have not been provided and a detail would need to be agreed before we approve this design.
- As works are being proposed on this access road, detailed proposals will need to be submitted for approval which include levels as it must be ensured that the works on the service road do not impact on the surrounding areas including the adopted highway and that the surface water in this area is adequately dealt with/discharged.

Servicing/Deliveries

- The applicant has not provided details of how the proposed properties will be serviced for parcel, deliveries of large items, etc. Couriers will almost always park as close as possible to the property being serviced as expected and therefore they are likely to park on the narrow access road. The applicant is required to demonstrate how this could work without causing an obstruction.
- Whilst the above may be acceptable, the applicant must demonstrate that a large panel van (Mercedes Sprinter Van LWB- as typically used by couriers in this area) will be able to safely service the properties, ensuring it can enter and exit the shared surface road in forward gear. The applicant should provide a servicing and delivery management plan to demonstrate this.

Surface Water Disposal

- It is unclear how the applicant is expecting to discharge private surface water from the development site. The hardstanding areas must be designed in a way to ensure private surface water is contained within the site and discharged appropriately. It is not permissible for the private surface water to run-off onto the shared surface road or into the highway drainage network.

Summary

- The local highway authority has identified a number of concerns within the key areas identified above where further information or clarification is required. As such I confirm that the Local Highway Authority cannot support this application in its current form unless the information listed above can be satisfactorily addressed or provided.

Amended comments received 16 August 2021

Access Visibility

SBC Highways and Transport have no objection to the proposed development on the basis of the visibility from the proposed vehicular access point, which is the proposed parking spaces proposed perpendicular to the access road.

The Proposed Layout (Drawing No. 983-001-005, dated 7th July 2021) demonstrates that only 2 metres x 11 metres visibility is available from the proposed parking spaces to the bend in the access road, which does not accord with the Manual for Streets Visibility requirements for a 30mph speed limit. However, the applicant has offered to provide speed control measures in the form of a 3m long 'speed table' to reduce vehicle speeds along the access road.

Vehicle speeds along the access road are likely to be low in the existing situation due to the narrow nature of the access road which measures 3.2m – 4.5m wide. Vehicle speeds are also likely to be low given the road is a dead end with vehicles either slowing down to park on their driveway or garage. Vehicles departing their driveway will have had minimal distance to gather speed when they pass the site access. The provision of a speed table will further ensure vehicles are travelling slowly on approach/departure from the proposed dwellings and from the existing dwellings.

The applicant has also demonstrated pedestrian visibility splays of 2m x 2m can be provided on drawing '1471 S04 P1 27.08.20'.

The provision of the speed table should be secured by a Section 278 Agreement / Minor Highway Works agreement with the Local Highway Authority.

Access Road

The LHA raised concerns relating to the lack of detail provided for the proposed access road becoming a shared surface. In response to these concerns, insufficient details have been provided.

The applicant has previously made reference to resurfacing the road however, no details of construction/materials have been submitted and considering that this is being considered as a shared surface road, the construction of the road must be suited to this use in accordance with guidelines.

As works are being proposed on this access road, detailed proposals will need to be submitted for approval which include levels as it must be ensured that the works on the service road do not impact on the surrounding areas including the adopted highway and that the surface water in this area is adequately treated and discharged appropriately. The shared surface must be suitably designed in accordance with the relevant guidance and standards for shared surfaces. DfT guidance requires us to ensure that all schemes are designed with the needs of different users in mind and satisfy their obligations under the equality's legislation.

SBC Highways and Transport recommend that further details of the surface road design are secured by planning condition and that the applicant enters into a Section 278 Agreement / Minor Highway Works agreement.

SBC Highways and Transport also recommend a condition requiring the provision of appropriate lighting along the shared surface access.

Car Parking

Further to comments from the LHA regarding increasing the width of bays sited next obstructions to 300mm, the width of the proposed parking space for Unit 3 has been widened to 2.9m and is therefore acceptable.

Swept path analysis was also requested for this parking space based on a DB32 large car. This has been provided (Drawing 1471 S04 P1 27.08.20) and this demonstrates that this parking space can be used.

Drainage

Concerns were raised by the LHA as to how private surface water would be discharged from the development site. The applicant has submitted drawing 1471 S05 P2 26.08.20 which shows that surface water is contained within the site and the provisional location of a SW attenuation tank in the rear garden of Unit 2 which will discharge to an existing mains SW system. Therefore, no private surface water will run-off onto the shared surface road or into the highway drainage network. This is therefore acceptable.

Details of surface water disposal for the access road have not been provided and these should be secured by planning condition.

Cycle Parking

The SBC Developers Guide: Part 3: Highways and Transport (2008) requires the provision of 1 secure, covered cycle parking space per dwelling to facilitate travel by sustainable travel modes. The proposed site plan does not label/identify cycle parking for each dwelling.

SBC Highways and Transport recommend that further details of cycle parking provision are secured by planning condition.

Servicing and Refuse Collection

SBC Highways and Transport have no objection to the proposed development on the basis of the servicing or refuse collection arrangements for the proposed dwellings.

The LHA previously requested that the applicant demonstrates that a large panel van (Mercedes Sprinter Van LWB- as typically used by couriers in this area) will be able to safely service the properties, ensuring it can enter and exit the shared surface road in forward gear. This has been provided and is acceptable.

It is anticipated that refuse collection would take place from the highway which is approximately 25m from the proposed refuse collection point and is therefore considered acceptable.

As previously commented, previous plans showed that the collection point would only accommodate 3 wheelie bins and the LHA requested that 6no 240l wheelie bins should be accommodated. Updated plans (Drawing 1471 S03 P2 27.08.20) now show that 6 wheelie bins can be accommodated, and this is therefore acceptable.

Summary and Conclusions

Subject to the applicant providing the requested information to allay my concerns, I confirm that I have no objection to this application from a transport and highway perspective.

6.2 Thames Water

Waste Comments

We would expect the developer to demonstrate what measures will be undertaken to minimise groundwater discharges into the public sewer. Groundwater discharges typically result from construction site dewatering, deep excavations, basement infiltration, borehole installation, testing and site remediation. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. Should the Local Planning Authority be minded to approve the planning application, Thames Water would like the following informative attached to the planning permission: "A Groundwater Risk Management Permit from Thames

Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. We would expect the developer to demonstrate what measures he will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 020 3577 9483 or by emailing trade.effluent@thameswater.co.uk. Application forms should be completed on line via www.thameswater.co.uk. Please refer to the Wholesale; Business customers; Groundwater discharges section.

With regard to SURFACE WATER drainage, Thames Water would advise that if the developer follows the sequential approach to the disposal of surface water we would have no objection. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. Should you require further information please refer to our website. <https://developers.thameswater.co.uk/Developing-a-large-site/Apply-and-pay-for-services/Wastewater-services>

Thames Water would advise that with regard to WASTE WATER NETWORK and SEWAGE TREATMENT WORKS infrastructure capacity, we would not have any objection to the above planning application, based on the information provided.

Water Comments

If you are planning on using mains water for construction purposes, it's important you let Thames Water know before you start using it, to avoid potential fines for improper usage. More information and how to apply can be found online at thameswater.co.uk/buildingwater.

On the basis of information provided, Thames Water would advise that with regard to water network and water treatment infrastructure capacity, we would not have any objection to the above planning application. Thames Water recommends the following informative be attached to this planning permission. Thames Water will aim to provide customers with a minimum pressure of 10m head (approx 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.

The applicant is advised that their development boundary falls within a Source Protection Zone for groundwater abstraction. These zones may be at particular risk from polluting activities on or below the land surface. To prevent pollution, the Environment Agency and Thames Water (or other local water undertaker) will use a tiered, risk-based approach to regulate activities that may impact groundwater resources. The applicant is encouraged to read the Environment Agency's approach to groundwater protection (available at <https://www.gov.uk/government/publications/groundwater-protection-position-statements>) and may wish to discuss the implication for their development with a suitably qualified environmental consultant.

6.3 Tree/landscape officer

The submitted ecology report identifies a number of trees within the gardens.

However there is no supporting tree survey and tree protection plan.

As the new buildings will have a considerable impact on the local area retaining trees will be vital to ensure that the impact of the proposal is minimised as much as possible. To achieve this supporting arboricultural survey in accordance with BS5837:2012 Trees in relation to design demolition and construction-recommendations must be submitted prior to any approval.

A landscape plan is also required that builds on the existing features to provide a comfortable relationship with the surrounding gardens and residential properties.

Further comments received 29th September 2020 in relation to the additional information stating that there are now no reasons for refusal. Should the application be approved then a landscaping condition is requested for the following reason:

To provide a high quality environment around the development and replace any trees that have been removed.

6.4 **Contamination officer**

Raised no objections subject to conditions.

6.5 **Environment Agency**

Thank you for consulting us with this application. The application site lies within Flood Zone 2, an area defined as being at medium risk of flooding according to the Planning Guidance. The site is also located within a Source Protection Zone 1 (SPZ).

Groundwater SPZs are areas of groundwater where there is a particular sensitivity to pollution risks due to the closeness of a drinking water source and how the groundwater flows.

They are used to protect abstractions used for public water supply and other forms of distribution to the public such as mineral and bottled water plants, breweries and food production plants. SPZ 1 represents the inner zone and are the most sensitive.

Environment Agency Position

We have no objections to the proposed development as submitted.
Flood Risk Standing Advice - Advice to LPA

The proposed development falls within Flood Zone 2, which is land defined in the [planning practice guidance](#) as being at risk of flooding.

We have produced a series of standard comments for local planning authorities and planning applicants to refer to on 'lower risk' development proposals. These comments replace direct case-by-case consultation with us. This proposal falls within this category.

These standard comments are known as Flood Risk Standing Advice (FRSA). They can be viewed at <https://www.gov.uk/guidance/flood-risk-assessment-for->

[planningapplications#when-to-follow-standing-advice](#)

We recommend that you view our standing advice in full before making a decision on this application.

I also enclose our Thames Area safe access and egress guidance note.
Sequential Test - Advice to LPA

In accordance with the National Planning Policy Framework (paragraph 158), development should not be permitted if there are reasonably available sites appropriate for the proposed development in areas with a lower probability of flooding.

It is for the local planning authority to determine if the sequential test has to be applied and whether or not there are other sites available at lower flood risk. Our flood risk standing advice reminds you of this and provides advice on how to apply the test.

6.6 **Lead Local Flood Authority**

Thank you for consulting us on the above planning application. Hampshire County Council has provided comments in relation to the above application as a consultant to Slough Borough Council for surface water drainage.

We have reviewed the following information in relation to the above planning application:

- Ambiental Phase 1 Flood Risk Assessment 4750_4026 V 2.1

In order for us to provide a substantive response, the following information is required:

There is no detail drainage strategy contained within the FRA. We will require a drainage layout, storage calculations, proposed flow rates and point of discharge. If soakaways are proposed soil infiltration testing will be required.

This response has been provided using the best knowledge and information submitted as part of the planning application at the time of responding and is reliant on the accuracy of that information.

Comments on amended information dated 25th September 2020:

Thank you for consulting us on the above planning application. Hampshire County Council has provided comments in relation to the above application as a consultant to Slough Borough Council for surface water drainage.

We have reviewed the following information in relation to the above planning application:

- Ambiental Phase 1 Flood Risk Assessment 4750_4026 V 2.1
- IBA Architects Surface Water drainage layout 1471 (S)05 P1

In order for us to provide a substantive response, the following information is

required:

We require storage calculations, proposed flow rates and justification, point of discharge and water quality information. Existing sewer information including depths and finished floor levels for the proposed units needs to be indicated to confirm if a gravity or pumped connection is proposed.

6.7 **Archaeology officer**

Thank you for consulting Berkshire Archaeology regarding the above application. Berkshire Archaeology is part of Reading Borough Council's Museum and Town Hall Services and provides historic environment advice to the five unitary authorities of Bracknell Forest Council, Reading Borough Council, Royal Borough of Windsor and Maidenhead, Slough Borough Council and Wokingham Borough Council.

There are potential archaeological implications associated with this proposed scheme. The site lies within the Thames valley. It therefore lies over the floodplain and gravel terraces which have been a focus of settlement, agriculture and burial from the earlier prehistoric period to the present day, as evidenced by data held on Berkshire Archaeology's Historic Environment Record.

Less than 80m south west from the proposal area, there is the known site of a mid to late Iron Age settlement which lies primarily within the land known as Agars Plough. This settlement was identified during an archaeological evaluation, but the limits of the prehistoric settlement are not thoroughly understood. A geophysical survey in the year 2000, of land to the west, showed that the site continued beyond the previously identified area, and as such has potential to extend to the location of these development proposals.

Therefore the application site falls within an area of archaeological significance and archaeological remains may be damaged by ground disturbance for the proposed development. It is therefore recommended that the following condition is applied should permission be granted in order to mitigate the impacts of development. This is in accordance with Paragraph 199 of the NPPF which states that local planning authorities should '*require developers to record and advance understanding of the significance of any heritage assets to be lost (wholly or in part) in a manner proportionate to their importance and the impact, and to make this evidence (and any archive generated) publicly accessible*'.

Condition

A) No development shall take place/commence until a programme of archaeological work including a Written Scheme of Investigation has been submitted to and approved by the local planning authority in writing. The scheme shall include an assessment of significance and research questions; and:

1. The programme and methodology of site investigation and recording
2. The programme for post investigation assessment
3. Provision to be made for analysis of the site investigation and recording
4. Provision to be made for publication and dissemination of the analysis and records of the site investigation
5. Provision to be made for archive deposition of the analysis and records of

the site investigation

6. Nomination of a competent person or persons/organisation to undertake the works set out within the Written Scheme of Investigation.

B) The Development shall take place in accordance with the Written Scheme of Investigation approved under condition (A).

The development shall not be occupied until the site investigation and post investigation assessment has been completed in accordance with the programme set out in the Written Scheme of Investigation approved under condition (A) and the provision made for analysis, publication and dissemination of results and archive deposition has been secured.

Reason: The site lies in an area of archaeological potential, particularly for, but not limited to, Prehistoric remains. The potential impacts of the development can be mitigated through a programme of archaeological work. This is in accordance with national and local plan policy.

In view of the nature and scale of the development and the low likelihood of the potential archaeology, should it exist, meriting preservation in situ, field evaluation through trial trenching would represent an appropriate initial phase of work in order to determine the archaeological potential and levels of previous truncation and the need for any further phases of work.

Berkshire Archaeology would be pleased to discuss the approach with the applicant or their archaeological consultant should permission be granted. If the applicant can demonstrate previous widespread impact on below ground deposits which specifically affect the archaeological potential, then this advice can be reviewed.

I trust this is satisfactory but if you have any questions or would like to discuss this in more detail then please do not hesitate to contact me.

6.8 **Ecology**

No comments received at the time of writing. Any comments received will be reported via the amendment sheet.

6.9 **Environmental noise**

No comments received at the time of writing. Any comments received will be reported via the amendment sheet.

7.0 **Policy Background**

7.1 Slough Local Development Framework Core Strategy 2006-2026 Development Plan Document policies:

- Core Policy 1 – Spatial Strategy
- Core Policy 3 - Housing Distribution
- Core Policy 4 - Type of Housing
- Core Policy 7 – Transport
- Core Policy 8 – Sustainability and the Environment
- Core Policy 9 – Natural and Built Environment

- Core Policy 10 – Infrastructure
- Core Policy 11 – Social Cohesiveness
- Core Policy 12 – Community Safety

Local Plan for Slough March 2004 policies:

- EN1 – Standard of Design
- EN2 - Extensions
- EN3 – Landscaping Requirements
- EN5 – Design and Crime Prevention
- H13 - Backland/Infill Development
- H14 - Amenity Space
- H15 – Residential Extensions
- T2 - Parking Restraint
- T8 - Cycling Network and Facilities

Slough Local Development Plan and the NPPF

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that applications for planning permission are determined in accordance with the development plan unless material considerations indicate otherwise. Annex 1 to the National Planning Policy Framework advises that due weight should be given to relevant policies in existing plans according to their degree of consistency with the Framework (the closer the policies in the plan to the policies in the Framework, the greater the weight that may be given). The revised version of the National Planning Policy Framework (NPPF) was published on 19th February 2019.

The National Planning Policy Framework 2021 states that decision-makers at every level should seek to approve applications for sustainable development where possible and planning law requires that applications for planning permission be determined in accordance with the development plan, unless material considerations indicate otherwise.

Following the application of the updated Housing Delivery Test set out in the National Planning Policy Framework 2021, the Local Planning Authority can not demonstrate a Five Year Housing Land Supply. Therefore, when applying Development Plan Policies in relation to the distribution of housing, regard will be given to the presumption in favour of sustainable development tilted in favour of the supply of housing as set out in Paragraph 11 of the National Planning Policy Framework 2021 and refined in case law.

Planning Officers have considered the revised National Planning Policy Framework 2021 which has been used together with other material planning considerations to assess this planning application.

7.2 Emerging Preferred Spatial Strategy for the Local Plan for Slough 2013-2036

On 1st November 2017 the Planning Committee approved further testing and consideration of the Emerging Preferred Spatial Strategy for the Local Plan for Slough 2013-2036.

7.3 On 26th August 2020 the Committee considered Local Plan Strategy Key Components. These key components are:

- Delivering major comprehensive redevelopment within the “Centre of Slough”;
- Selecting other key locations for appropriate sustainable development;
- Enhancing our distinct suburbs, vibrant neighbourhood centres and environmental assets;
- Protecting the “Strategic Gap” between Slough and Greater London;
- Promoting the cross border expansion of Slough to meet unmet housing needs.

8.0 **Planning Considerations**

8.1 The planning considerations for this proposal are:

- Principle of the proposal
- Design and impact on the character and appearance of the area
- Impact on neighbouring amenity
- Living conditions of future occupiers
- Highways and transport
- Contamination
- Trees and ecology
- Drainage Considerations

9.0 **Principle of Development**

9.1 Given the absence of a five year supply in housing, the LPA must undertake an exercise in judgement in determining the appropriate balance of considerations as to whether the adverse impacts of the development would significantly and demonstrably outweigh the benefits when assessed against the policies in the Local Development Plan and the National Planning Policy Framework 2019 taken as a whole. It is required to assess whether the proposed development is sustainable as defined by the NPPF 2021.

9.2 Core Policy 4 of The Slough Local Development Framework Core Strategy 2006-2026 Development Plan Document states that in urban areas outside the town centre, new residential development will predominantly consist of family housing. The Berkshire Strategic Housing Market Assessment has identified the need for family housing which reflects the disproportionate number of flats which have been completed in recent years as a result any development within the urban area should consist predominantly of family housing.

9.3 The Core Strategy defines family housing. As of 1st November 2018 the definition has been updated and is shown below:

‘A fully self-contained dwelling with a minimum gross internal floor area of 79 square metres, that has direct access to a private garden. Comprises a minimum of two bedrooms and may include detached, semi-detached, terraced and town house dwellings but not flats and maisonettes.’

- 9.4 The proposed dwellings have two/three bedrooms, have an internal floor area in excess of 79sq.m and have direct access to a private garden. The proposed development would therefore provide family housing to the required standard.
- 9.5 Given there would be no net loss of family housing and the weight of balance is tilted towards the supply of housing, the principle of this housing type would be acceptable subject to complying with the relevant planning considerations which are assessed below.
- 9.6 Policy H13 (Backland Development) of Slough Local Plan (2004) sets the requirements to allow appropriate backland development and criteria to resist inappropriate development of residential gardens as backland/infill developments such as this application for the proposed development. The Council has to consider whether the proposal would be sustainable development, and part of that assessment is whether development would contribute to the protection and enhancement of the natural, built and historic environment. It is therefore considered that an assessment should be made on whether the development would cause harm to the local area.
- 9.7 The proposal seeks the subdivision of a plot of land to provide three new family dwellings. The provision of three additional family houses would generally contribute to the housing provision within the Borough.
- 9.8 However, in order to comply with policy H13 of the Local Plan, the proposed development needs to meet additional criteria. These will be discussed within the remainder of the report.

10.0 Design and Impact on Appearance and Character of the area

- 10.1 Policy EN1 of the Local Plan outlines that development proposals are required to reflect a high standard of design and must be compatible with and/or improve their surroundings in terms of scale, height, massing, layout, siting, building form and design, architectural style, materials, access points, visual impact, relationship to nearby properties, relationship to mature trees, and relationship to water course. Poor designs which are not in keeping with their surroundings and schemes that overdevelop the site will not be permitted.
- 10.2 Of particular relevance is policy H13 of the Local Plan. This is shown below:

'Proposals for small scale infilling, including backland development, will not be permitted unless they comply with all of the following criteria:

- a) the type, design, scale and density of the proposed new dwelling or dwellings are in keeping with the existing residential area;*
- b) appropriate access, amenity space and landscaping are provided for the new dwellings;*
- c) appropriate car parking provision is made in line with the aims of the integrated transport strategy;*
- d) the scheme is designed so that existing residential properties retain appropriate garden areas, they do not suffer from overlooking or loss of privacy, and there is no substantial loss of amenity due to the creation of new access roads or parking areas;*
- e) the proposal is not located within a residential area of exceptional character; and*

f) the proposal optimises the potential for more comprehensive development of the area and will not result in the sterilisation of future residential land.'

10.3 Core Strategy Policy 8 states that all development in the borough shall be sustainable, of a high quality design, improve the quality of the environment and address the impact of climate change. Core Policy 8 outlines:

'All development will:

- a) Be of a high quality design that is practical, attractive, safe, accessible and adaptable;*
- b) Respect its location and surroundings;*
- c) Provide appropriate public space, amenity space and landscaping as an integral part of the design; and*
- d) Be in accordance with the Spatial Strategy in terms of its height, scale, massing and architectural style.'*

10.4 Although the proposed development is located on land to the rear of numbers 41, 43 and 45 The Myrke, the access point is taken from the east from a private road. The proposed dwellings would face this road, creating a new active frontage. There are two existing rear developments at 71 and 73 The Myrke (forming 83a and 83b The Myrke). These retain a frontage on The Myrke due to the bend in the road.

10.5 The dwellings provide a frontage to a built up area that already has residential units in the area. The siting of the dwelling will not therefore be out of character of the area due to the pattern of existing established development. In this respect, the proposed development is acceptable.

10.6 In terms of scale and form the proposed scheme has been amended since its original submission to reduce the scale of proposals and bring them more in keeping with the character of the area. As a result the proposed development is considered to provide 3no residential units that are reflective of the scale of development in the area.

10.6 On the basis of the considerations above, it is considered that the proposed development will accord with policies EN1 of the Local Plan and CP8 of the Core Strategy and the requirements of the NPPF 2019.

11.0 Impact on neighbouring amenity

11.1 Policy CP8 of the Core Strategy requires that the design of all new development should respect the amenities of adjoining occupiers. The proposed development does not include any extensions to the building or any additional openings. The replacement of external cladding will have no harmful impact on the amenities of neighbouring occupiers and the proposed development is therefore considered to comply with policy CP8 of the Core Strategy and the requirements of the NPPF 2019.

11.2 The proposed dwellings have been sited on the plot in locations to establish distances to other existing dwellings. Units 1 and 2 are set (at their closest point) 21m from the rear of 45 The Myrke to the west. Unit 3 is separated from 43 The Myrke by a distance of 27.8m and from 41 The Myrke by a distance of 31.6m. These distance are, in planning terms, considered to provide suitable

separation between the buildings to the extent that there is not considered to result in adverse overlooking impact to neighbouring windows.

- 11.3 The proposed dwellings all have a garden depth of 9m which, when coupled with screening from existing and proposed landscaping on the western boundaries, would provide suitable distance to ensure there is not adverse overlooking impact on the rear gardens of neighbouring properties to the west.
- 11.4 In terms of potential overlooking to the north and south, the plans show that any side windows proposed at first floor level would serve bathrooms to the respective dwellings therefore it is expected that these would be fitted with obscure glazing. The side windows would have an outlook onto neighbouring gardens but the potential for overlooking can be addressed through inclusion of conditions that require these windows to be fitted with obscure glazing and to be non opening up to an internal height of 1.7m.
- 11.5 To the east the outlook is towards hardstanding that serves existing garages and screen landscaping beyond that. To the southeast is a two storey block of residential units which have garden areas that face north and are adjacent to the access road to the application sites. Objection has been received from residents of these units on the grounds of harm to privacy. The proposed dwellings will face towards the rear gardens of these neighbouring units but there is considered to be a suitable distance from the outlooks to ensure there is no direct overlooking. Furthermore the garage blocks act to screen the gardens from the proposed units. There is a suitable distance to ensure there would be no loss of light or overshadowing and the orientation of the neighbouring windows means that the development would not appear overbearing from the neighbouring units either.
- 11.6 The siting of the proposed dwellings and their scale are such that there is not considered to be any significant impact on the extent of light to any neighbouring properties and no adverse impact would occur through overshadowing.
- 11.7 The proposal is therefore considered to be acceptable in light of Core Policy 8 of the Local Development Framework Core Strategy and Policies EN1 and EN2 of the Adopted Local Plan.

12.0 Living conditions for future occupiers of the development

- 12.1 The NPPF states that planning should always seek to secure a quality design and a good standard of amenity for all existing and future occupants of land and buildings
- 12.2 Core policy 4 of Council's Core Strategy seeks high density residential development to achieve "a high standard of design which creates attractive living conditions."
- 12.3 The dwellings as proposed are acceptable when considered against the nationally prescribed space standards for dwelling which is acceptable. Each dwelling also has a suitably sized garden which ensures acceptable private amenity space is given as well.
- 12.4 The application is considered to be acceptable in light of the requirements of

the NPPF, Core policy 4 of Council's Core Strategy, and Policy H14 of the Adopted Local Plan.

13.0 Highways and Parking

- 13.1 The National Planning Policy Framework states that planning should seek to development is located where the need to travel will be minimised and the use of sustainable transport modes can be maximised. Development should be located and designed where practical to create safe and secure layouts which minimise conflicts between traffic and pedestrians. Where appropriate local parking standards should be applied to secure appropriate levels of parking. This is reflected in Core Policy 7 and Local Plan Policies T2 and T8. Paragraph 32 of the National Planning Policy Framework states that 'Development should only be prevented or refused on transport grounds where the residual cumulative impacts of development are severe'.
- 13.2 The Highways Officer has made a number of comments on the application and the applicant submitted amended plans as a result to take the comments on board. As a result of the revised plans there are now no objections to the scheme in highways terms.
- 13.3 It is noted that a number of objections have been received on highways grounds relating to parking, access, vehicle movement and emergency services.
- 13.4 In respect of the parking numbers, each unit has two parking spaces per unit and this is considered to be acceptable in planning terms. The parking spaces have been increased in size during this application to ensure they are usable spaces. It is noted that there is no visitor parking provided within the application site and while this would have been ideal the scale of development proposed and the size of the site means that the absence of visitor spaces would not, when considering the wider planning balance, render the scheme unacceptable in planning terms. No details of cycle parking have been provided but it is considered that there is space in the site for this to be provided in principle and a requirement of 1 cycle space per unit can be secured through condition.
- 13.5 The proposed access road will be subject to a notable intensification of use. It is single width and it would benefit from improvement. The access road is outside of the application site and works cannot be secured by standard condition or S106 agreement however it is possible to use a Grampian style condition to secure these works. Grampian conditions are conditions that require an applicant to complete a set off off-site works before any development on the application site can be commenced. Improvement to the access road for the new residents and those accessing the garages is considered to be reasonable and necessary to make the scheme acceptable in planning terms and therefore the condition has been included.
- 13.6 In respect of visibility, the amended plans that have been received demonstrate that suitable visibility can be achieved for all units on the basis that a speed table is to be installed on the access to control vehicle speeds. It is noted that vehicle speeds along the access road are likely to be low given its width and the fact that it is a dead end.

- 13.7 In respect of servicing access and refuse collection the amended details have demonstrated that a larger van would be able to enter and leave the access in a forward gear. The access is too narrow for a council waste service vehicle to enter however it is noted that bin collection would be done on the roadside at The Myrke which implies a 25m drag distance for bins, which is considered to be acceptable.
- 13.8 With regards to access for emergency vehicles the information submitted shows that a larger vehicle that would be similar to an ambulance can enter and leave the access in a forward gear. In respect of fire services the access is probably too narrow for this however it is not essential for access to be provided. In circumstance where there are access problems, Building Regs will require dwellings to be fitted with sprinkler systems and this would be the case with this proposal. In any case it is a Building Regs matter and, for planning purposes, there is no highways harm from such circumstances.
- 13.9 On the basis of the considerations made above, there are no highways impacts that would result in significant and demonstrable harm that would, in itself warrant a reason to refuse planning permission. Outstanding matters and implementation requirements can be secured by condition and are included as part of the recommendation.

14.0 Contamination

- 14.1 Core Policy 8 (Sustainability and the Environment) of the SBC's Core Strategy Document states that development shall not 'cause contamination or deterioration in land, soil or water quality' nor shall development occur on polluted land unless appropriate mitigation measures are employed.
- 14.2 The application site comprises residential gardens and the risk of contamination is therefore considered to be low. There is nothing in the planning history to suggest previous uses that could pose risks and no objections are raised as a result.

15.0 Trees and Ecology.

- 15.1 Paragraph 17 of the NPPF 2019 states that when determining planning applications, if significant harm to biodiversity cannot be avoided or adequately mitigated or as a last resort compensated for then planning permission should be refused. It also states that opportunities to incorporate biodiversity improvements in and around the developments should be encouraged, especially where this can secure measurable net gains for biodiversity.
- 15.2 The loss of garden area would result in a loss of soft landscape on the site. It is noted that there are no trees of significance lost as a result of the scheme and suitable distances are established between other trees on neighbouring sites.
- 15.3 The application scheme gives the opportunity to require the provision of bat and bird boxes on the proposed dwellings which would result in a demonstrable net gain in biodiversity and these can be secured by condition which means that there are no objections in respect of trees and biodiversity.

16.0 Drainage considerations

- 16.1 The site is located within flood zone 2 and the Environment Agency has raised no objections advising that their standing advice should be followed and the applicant's Flood Risk Assessment confirms that said advice can be followed. The applicant undertook a sequential test in accordance with the requirements of the NPPF and its findings are accepted..
- 16.2 Thames Water has considered the impact of the proposal on the surface water drainage, foul drainage, the waste water network and water availability. The comments confirm that there is capacity in the sewerage and water networks to accommodate the development without an adverse impact.
- 16.3 The development would increase the extent of hard surfaces on the site but, given the comments from Thames Water, there is not considered to be an impact that would adversely affect the area.

17.0 Archaeology

- 17.1 The Archaeologist has identified that there may be archaeological remains on the site and that a condition is requested to assess this. This is considered to be reasonable and has been included as part of the recommendation.

18.0 Planning Balance

- 18.1 In the application of the appropriate balance it is considered that there are significant benefits to be had through the provision of housing during a time that the Council is unable to demonstrate a deliverable 5 year housing land supply. It is also noted that conditions can secure a net gain in biodiversity. It is noted that the access is not ideal and a number of points have been raised in objection to the application from neighbouring residents. However, as the report has considered, none of the impacts raised through objection are considered to have a significant adverse impact that would result in harm that would outweigh the benefits identified. .
- 18.2 On balance it is recommended that planning permission should be granted in this case as the benefits significantly and demonstrably outweigh any adverse impacts and conflicts with specific policies in the NPPF.

19.0 Process

- 19.1 It is the view of the Local Planning Authority that the proposed development does improve the economic, social and environmental conditions of the area for the reasons given in this notice and it is in accordance with the National Planning Policy Framework.

20.0 PART C: RECOMMENDATION

- 20.1 Having considered the relevant policies set out above, comments from consultees and neighbours representations as well as all relevant material considerations it is recommended the application be **approved** subject to the following conditions.

21.0 PART D: CONDITIONS

CONDITIONS:

1. The development hereby permitted shall be commenced within three years from the date of this permission.

REASON To prevent the accumulation of planning permissions, and to enable the Council to review the suitability of the development in the light of altered circumstances and to comply with the provisions of Section 91 of the Town and Country Planning Act 1990.

2. The development hereby approved shall be implemented only in accordance with the following plans and drawings hereby approved unless otherwise agreed in writing by the Local Planning Authority.

- (a) Drawing No. 18039-18-01, Dated 09/02/2018, Recd On 29/06/2018
- (b) Site and Location Plan No. 00023H Rev: 0, Dated 28/05/2018, Recd On 29/06/2018
- (c) Drawing No. 00023E Rev: 0, Dated 28/05/2018, Recd On 29/06/2018
- (d) Drawing No. 00023F Rev: 0, Dated 28/05/2018, Recd On 29/06/2018
- (e) Drawing No. 00023G Rev: 1, Dated 20/08/2018, Recd On 21/08/2018
- (f) Drawing No. 00023H Rev: 0 Entitled 'Landscaping', Dated 28/05/2018, Recd On 29/06/2018
- (g) Drawing No. 00023H2 Rev: 0, Dated 28/05/2018, Recd On 29/06/2018
- (h) Drawing No. 00023I Rev: 0, Dated 28/05/2018, Recd On 29/06/2018
- (i) Drawing No. 00023J Rev: 0, Dated 28/05/2018, Recd On 29/06/2018
- (j) Drawing No. 00023K Rev: 0, Dated 28/05/2018, Recd On 29/06/2018
- (k) Undated/unnumbered Design & Access Statement by IE architecture
- (l) 7no. undated/unnumbered images on A2 sheets illustrating the proposed scheme
- (m) Drawing No. 983-001 Rev 005 dated 07/07/2021, received 12/08/2021

REASON To ensure that the site is developed in accordance with the submitted application and to ensure that the proposed development does not prejudice the amenity of the area and to comply with the Policies in the Development Plan.

3. No development above damp proof course level shall take place until details have been submitted to and approved in writing by the Local Planning Authority of the proposed external facing materials on the dwellings hereby approved. The development shall be carried out in accordance with the details approved unless otherwise agreed in writing with the Local Planning Authority.

REASON To ensure a satisfactory appearance of the development so as not to prejudice the visual amenity of the locality in accordance with Policy EN1 of The Adopted Local Plan for Slough 2004.

4. No window(s), other than those hereby approved, shall be formed in the elevations of the development hereby approved without the prior written approval of the Local Planning Authority.

REASON To minimise any loss of privacy to occupiers of adjoining

residential properties in accordance with Policy H15 of The Adopted Local Plan for Slough 2004.

5. The first floor windows on the north and south elevation shown on drawing no. 1471 (0)02 P3 and the first floor window on the north elevation shown on drawing no. 1471 (0)04 P3 shall be glazed in obscure glass and shall be non-opening below a height of 1.7 metres measured from the internal finished floor level. The windows shall not thereafter be altered in any way without the prior written approval of the Local Planning Authority.

REASON To minimise any loss of privacy to adjoining occupiers in accordance with Policy H15 of The Adopted Local Plan for Slough 2004

6. The development hereby permitted shall not be occupied until details of the proposed boundary treatment including position, external appearance, height and materials have been submitted to and approved by the Local Planning Authority. The approved details shall be carried out prior to the first occupation of the development and retained at all time in the future, unless otherwise agreed in writing by the Local Planning Authority.

REASON In the interests of the visual amenity of the area and in accordance with Core Policy 8 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008, Policy EN3 of The Adopted Local Plan for Slough 2004, and the requirements of the NPPF 2018.

7. Notwithstanding the terms and provisions of the Town & Country Planning (General Permitted Development) (England) Order 2015 (or any order revoking and re-enacting that Order), Schedule 2, Part 1, Classes A, B, C, D, E and G, no extension to the house hereby permitted or buildings or enclosures shall be erected constructed or placed on the site without the express permission of the Local Planning Authority.

REASON In the interests of design and amenity in accordance with Policies EN1 and H14 of The Adopted Local Plan for Slough 2004.

8. No dwelling hereby approved shall be occupied on site until a detailed landscaping and tree planting scheme has been submitted to and approved in writing by the Local Planning Authority. This scheme should include the trees and shrubs to be retained and/or removed and the type, density, position and planting heights of new trees and shrubs.

The approved scheme shall be carried out no later than the first planting season following completion of the development. Within a five year period following the implementation of the scheme, if any of the new or retained trees or shrubs should die, be removed or become seriously damaged or diseased, then they shall be replaced in the next planting season with another of the same species and size as agreed in the landscaping tree planting scheme by the Local Planning Authority.

REASON In the interests of the visual amenity of the area and in accordance with Policy EN3 of The Adopted Local Plan for Slough 2004.

9. None of the dwellings hereby approved shall be occupied until details have been submitted to and approved in writing by the Local Planning Authority

of the provision and location of 3no bat/bird boxes within the application site. The works shall be carried out in accordance with the approved details and be in place prior to first occupation and thereafter be retained.

REASON: To demonstrate a net gain in biodiversity in the interests of paragraph 17 of the National Planning Policy Framework 2019.

10. No part of the development shall be occupied until the new means of access to each unit has been sited and laid out in accordance with the approval plans and constructed in accordance with Slough Borough Council's Adopted Vehicle Crossover Policy.

REASON To ensure that the proposed development does not prejudice the free flow of traffic or conditions prejudicial of general safety along the neighbouring highway in accordance with Policy T3 of The Adopted Local Plan for Slough 2004.

11. No vehicle access gates, roller shutters doors or other vehicle entry barriers or control systems shall be installed without first obtaining permission in writing from the Local Planning Authority.

REASON: In order to minimise danger, obstruction and inconvenience to users of the highway and of the development.

12. No development shall take place until details have been submitted to and approved in writing by the Local Planning Authority of proposed works to resurface the existing access road that runs on the eastern side of the site to the point it joins the highway on The Myrke. The works shall be proposed to accord with Slough Borough Council's Adopted Vehicle Crossover Policy. The works shall be carried out in accordance with the approved details and be completed prior to the commencement of any development on site.

REASON: To ensure the access road from The Myrke is of an acceptable standard due to increased usage and to continue to allow access to the dwellings and adjacent garages in the interests of Policy T3 of The Adopted Local Plan for Slough 2004.

13. No other part of the development shall be occupied until the visibility splays shown on the approved drawings have been provided on both sides of the access and the area contained within the splays shall be kept free of any obstruction exceeding 600 mm in height above the nearside channel level of the carriageway.

REASON: To provide adequate intervisibility between the access and the existing public highway for the safety and convenience of users of the highway and of the access.

14. The scheme for parking and manoeuvring indicated on the submitted plans shall be laid out prior to the initial occupation of the development hereby permitted and that area shall not thereafter be used for any other purpose.

REASON: To enable vehicles to draw off, park and turn clear of the highway to minimise danger, obstruction and inconvenience to users of the adjoining highway.

15. The development shall not begin until details of the disposal of surface water from the highway have been approved in writing by the Local Planning Authority and no dwelling shall be occupied until the works for the disposal of surface water have been constructed in accordance with the approved details. No surface water from the development shall drain onto the public highway.

REASON: To minimise danger and inconvenience to highway users.

16. No part of the development shall be occupied until covered and lockable cycle parking stores with minimum dimensions of 2m in length x 2m in height and 1m in width are provided for each dwelling. The cycle parking shall be provided in accordance with these details and shall be retained at all times in the future for this purpose.

REASON: To ensure that there is adequate cycle parking available at the site in accordance with Policy T8 of The Local Plan for Slough 2004, and to meet the objectives of the Slough Integrated Transport Strategy.

17. No part of the development shall be occupied commence until suitable bin storage area has been provided in accordance with the standards set out in the Slough Developers Guide.

REASON: To ensure that adequate refuse storage is provided to serve the development

18. Development works shall not commence until a Phase 1 Desk Study (DS) has been submitted to and approved in writing by the Local Planning Authority. The Phase 1 Desk Study shall be carried out by a competent person in accordance with Government, Environment Agency and Department for Environment, Food and Rural Affairs (DEFRA) guidance and approved Codes of practices, including but not limited to, the Environment Agency model procedure for the Management of Land Contamination CLR11 and Contaminated Land Exposure Assessment (CLEA) framework, and CIRIA Contaminated Land Risk Assessment Guide to Good Practice C552. The Phase 1 Desk Study shall incorporate a desk study (including a site walkover) to identify all potential sources of contamination at the site, potential receptors and potential pollutant linkages (PPLs) to inform the site preliminary Conceptual Site Model (CSM) and Preliminary Risk Assessment (PRA).

REASON: To ensure that the site is adequately risk assessed for the proposed development and in accordance with Policy 8 of the Core Strategy 2008.

19. Should the findings of the Phase 1 Desk Study approved pursuant to the Phase 1 Desk Study condition identify the potential for contamination, development works shall not commence until an Intrusive Investigation Method Statement (IIMS) has been submitted to and approved in writing by the Local Planning Authority. The IIMS shall be prepared in accordance with current guidance, standards and approved Codes of Practice including, but not limited to, BS5930, BS10175, CIRIA 665 and BS8576. The IIMS shall include, as a minimum, a position statement on the available and previously completed site investigation information, a rationale for the

further site investigation required, including details of locations of such investigations, details of the methodologies, sampling and monitoring proposed.

REASON: To ensure that the type, nature and extent of contamination present, and the risks to receptors are adequately characterised, and to inform any remediation strategy proposal and in accordance with Policy 8 of the Core Strategy 2008.

20. Development works shall not commence until a Quantitative Risk Assessment (QRA) has been prepared for the site, based on the findings of the intrusive investigation. The risk assessment shall be prepared in accordance with the Contaminated Land report Model Procedure (CLR11) and Contaminated Land Exposure Assessment (CLEA) framework, and other relevant current guidance. This must first be submitted to and approved in writing by the Local Planning Authority and shall as a minimum, contain, but not limited to, details of any additional site investigation undertaken with a full review and update of the preliminary Conceptual Site Model (CSM) (prepared as part of the Phase 1 Desk Study), details of the assessment criteria selected for the risk assessment, their derivation and justification for use in the assessment, the findings of the assessment and recommendations for further works. Should the risk assessment identify the need for remediation, then details of the proposed remediation strategy shall be submitted in writing to and approved by the Local Planning Authority. The Site Specific Remediation Strategy (SSRS) shall include, as a minimum, but not limited to, details of the precise location of the remediation works and/or monitoring proposed, including earth movements, licensing and regulatory liaison, health, safety and environmental controls, and any validation requirements.

REASON: To ensure that potential risks from land contamination are adequately assessed and remediation works are adequately carried out, to safeguard the environment and to ensure that the development is suitable for the proposed use and in accordance with Policy 8 of the Core Strategy 2008.

21. No development within or adjacent to any area(s) subject to remediation works carried out pursuant to the Phase 3 Quantitative Risk Assessment and Site Specific Remediation Strategy condition shall be occupied until a full Validation Report for the purposes of human health protection has been submitted to and approved in writing by the Local Planning Authority. The report shall include details of the implementation of the remedial strategy and any contingency plan works approved pursuant to the Site Specific Remediation Strategy condition above. In the event that gas and/or vapour protection measures are specified by the remedial strategy, the report shall include written confirmation from a Building Control Regulator that all such measures have been implemented.

REASON: To ensure that remediation work is adequately validated and recorded, in the interest of safeguarding public health and in accordance with Policy 8 of the Core Strategy 2008.

22. A) No development shall take place/commence until a programme of archaeological work including a Written Scheme of Investigation has been submitted to and approved by the local planning authority in writing. The

scheme shall include an assessment of significance and research questions; and:

1. The programme and methodology of site investigation and recording
2. The programme for post investigation assessment
3. Provision to be made for analysis of the site investigation and recording
4. Provision to be made for publication and dissemination of the analysis and records of the site investigation
5. Provision to be made for archive deposition of the analysis and records of the site investigation
6. Nomination of a competent person or persons/organisation to undertake the works set out within the Written Scheme of Investigation.

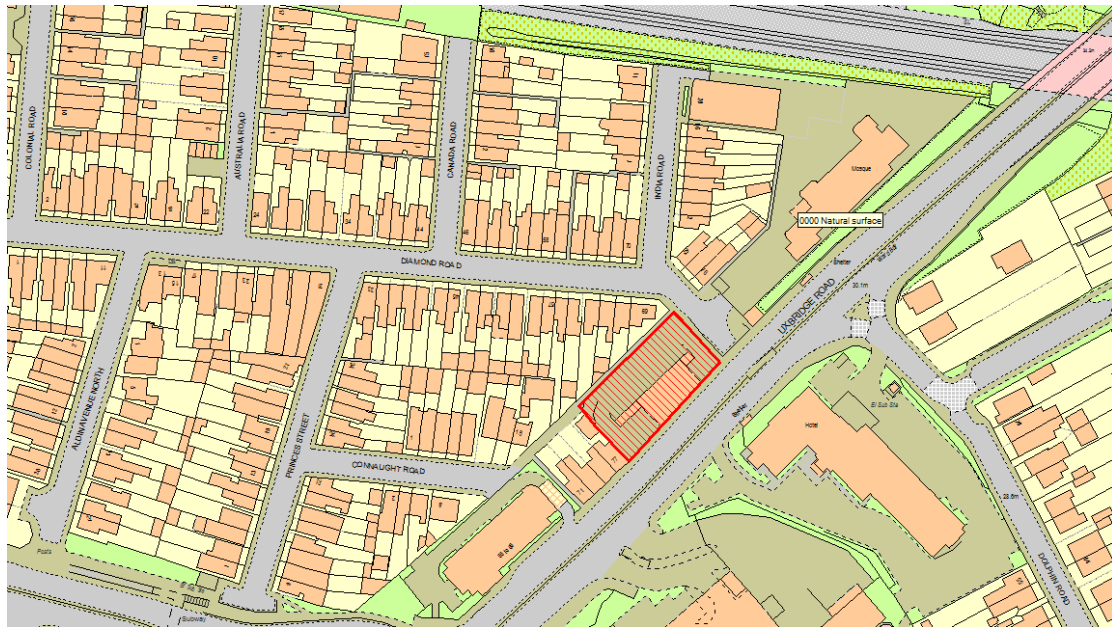
B) The Development shall take place in accordance with the Written Scheme of Investigation approved under condition (A).

The development shall not be occupied until the site investigation and post investigation assessment has been completed in accordance with the programme set out in the Written Scheme of Investigation approved under condition (A) and the provision made for analysis, publication and dissemination of results and archive deposition has been secured.

Reason: The site lies in an area of archaeological potential, particularly for, but not limited to, Prehistoric remains. The potential impacts of the development can be mitigated through a programme of archaeological work. This is in accordance with national and local plan policy.

Registration Date:	28-Jan-2021	Application No:	P/01303/018
Officer:	Alex Harrison	Ward:	Central
Applicant:	Mr. M Afzal, Slough Islamic Trust	Application Type:	Major
		13 Week Date:	29 April 2021
Agent:	Mr. Robert Farr, Lines and Squares Ltd 107, Lansdowne Avenue, Stockwell, London, SW8 2PD		
Location:	79-83, Uxbridge Road, Slough, SL1 1SG		
Proposal:	Redevelopment of former Car Wash to provided a new building up to 4 storeys in height with additional accommodation in roof all to be used as a learning and non-residential institution, for, or in connection with, public worship or religious instruction (F1.f Use Class) with associated car parking to be used in conjunction with Diamond Road Mosque.		

Recommendation: Approve, subject to conditions



P/01303/002 – 79-83, Uxbridge Road, Slough, SL1 1SG

1.0 SUMMARY OF RECOMMENDATION

- 1.1 Under the current constitution this application is being brought to Committee for decision as the application is for major development.
- 1.2 Having considered the relevant policies set out below, the representations received from all consultees and neighbouring residents, as well as all other relevant material considerations, it is recommended that the application be approved subject to conditions.

PART A: BACKGROUND

2.0 Proposal

- 2.1 Planning permission is sought for the redevelopment of the site to provide a 3/4 storey building fronting Uxbridge Road that would provide 'support services' for the Slough Islamic Trust mosque. The accommodation will consist of 4 classrooms and two larger communal teaching spaces, ancillary breakout spaces and other facilities such as toilets and a small kitchenette. It is anticipated that the classrooms will be used by between 6 and 12 people with up to 18 people in the larger communal teaching spaces some, the estimated numbers of people in the building being around 80 people at any one time.
- 2.2 From the Uxbridge Road frontage the building reads as a 2-3 storey structure however given the site levels the Diamond Road frontage will see it as a 4 storey building. The lower ground and ground floor are proposed to provide parking with 46 spaces provided in total. The parking will provide overflow spaces for the mosque. Access is to be gained from Diamond Road and egress will be made onto Uxbridge Road.
- 2.3 The building will be faced with brickwork on the elevations and the roof covering will be zinc.
- 2.4 Following discussions with the applicant it has been advised that the mosque is used more greatly than it used to be and notable there is an increased requirement for funerals which has resulted in compromised space for the education aspect of the mosque and the proposal will provide that facility.

3.0 Application Site

- 3.1 The application site is an area of previously developed land which is currently vacant and last operated as a car wash. The site is located on Uxbridge Road in Slough, one of the main routes running into the centre of the town. Access can be gained to the site via Uxbridge Road and also at the end of Diamond Road to the west of the site.
- 3.2 The site slopes from southeast to northwest and land levels are established with two principal flat areas stepped with a retaining wall and sloped vehicles access

either side. The site is unused currently and has structures relating to its last use as car park on it.

3.3 The wider character of the area is relatively mixed. The predominant use is residential with established housing to the east but there are also services such as retail units, a supermarket and hotel nearby also. The mosque, to which this application is associated with, is to the immediate northeast of the site.

4.0 Site History

4.1 The following accounts for the planning history of the site:

4.2 P/01303/017

Retention of hand car wash and valeting (sui generis) with associated buildings and access.

Never formally registered

4.3 P/01303/016

Change of use to hand car wash and valeting (Use Class Sui Generis). Erection of fabric canopy for car washing and valeting and portable cabin for office and waiting area.

Refused 18/02/2018

4.4 P/01303/015

Lawful development certificate for the use of the site as a car park.

Approved 01/07/2017

4.5 P/01303/014

Change of use to hand car wash and valeting (Use Class Sui Generis). Erection of fabric canopy for car washing and valeting and portable cabin for office and waiting area

Approved 18/07/2017

4.6 P/01303/013

Erection of a three / four storey block comprising mosque office and storage at loft level, 12 no two bedroomed flats, 1 no. One bedroom and student accommodation (7 no. Rooms plus communal facilities) at first and second floor level, 2 retail units security office and 17 no. Parking spaces and cycle parking at ground floor level, 25 no. Parking spaces (including 1 disabled), retail service bay, refuse storage and cycle parking at basement level and ramped access between Parking levels with storage space under, and limited vehicular egress on to uxbridge road.

Approved 30/09/2010

4.7 P/01303/012

Erection of part four storey/ part two storey block comprising 8no. Two bedroom flats and 6no. One bedroom flats, 3 retail units together with parking for 25 cars at lower ground floor level and limited vehicular egress onto Uxbridge road.

Approved 15/11/2005

4.8 P/01303/011

Erection of part four storey/ part three storey block comprising 10 no. Two bedroom flats and 5 no. One bedroom, 3 no. Retail units and underground parking

for 25 no. Cars
Withdrawn

4.9 P/01303/009
Change of use from petrol filling station to sale of second hand tyres and tyre fitting
Approved 30/05/1996

5.0 Neighbour Notification

5.1 In accordance with Article 15 of The Town and Country Planning (Development Management Procedure) (England) Order 2015 (as amended) a site notice was displayed outside the site on 02/02/21. The application was advertised as a major application in the The Slough Express

5.2 26 letters of objection were received, with 1 letter subsequently being withdrawn. The planning objections raised are summarized as follows:

- Significant parking problems in the area that will be made worse.
- Additional traffic will cause congestion on nearby streets and affecting nearby shops.
- There is no need to expand the mosque.
- Will lead to increased population in the area.
- Overlooking to 1 India Road and the general area
- Noise pollution from activities and vehicles.
- The building is out of character with the area and overly tall.
- No clarification of opening hours are a concern
- No details of how waste will be managed.
- No details of how foul sewage will be dealt with.
- Use of the building will not be available for the whole community.

5.3 In addition, 3 letters of support have been received raising the following points:

- Will provide a useful facility
- Additional parking will help the area
- The design will blend well with its surroundings.

6.0 Consultations

6.1 Transport and Highways

Following the receipt of additional information:

Vehicular Access

It is proposed to provide access via Diamond Road utilising the existing crossover. A one-way system is proposed, with vehicles exiting onto Uxbridge Road via the retention of an existing access with Uxbridge Road. It is proposed no vehicles will enter the site from Uxbridge Road.

At the request of SBC Highways and Transport, the applicant has provided Drawing No.SP01-Rev-A which demonstrates the kerb alignment for the proposed

access will prevent cars entering the site from the Uxbridge Road. The applicant should provide details of signage directing vehicles to turn left when departing the site at detailed design stage, which should be provided in accordance with the TSRGD.

Drawing No. SK-03-Rev-A demonstrates that a visibility splay of 2.4 metres x 40 metres is available looking to the right along the Uxbridge Road from the existing site access junction. The visibility splay of 2.4m x 40m is appropriate for vehicle speeds of 29mph. It is understood that this existing site access currently serves as both as ingress/egress for the existing site and there is no recorded accident problem associated with vehicles exiting the site using the existing level of visibility from the access.

The applicant's transport consultant has stated that if the setback distance is reduced to 2m, then emerging drivers would have full visibility to the A412 Uxbridge Road / A4 Wellington Street Roundabout. No plan has been provided which demonstrates this level of visibility is available.

It has been agreed that a Stage 1 Road Safety Audit for the proposed access and layout will be completed post determination, with the audit completed in accordance with DMRB GG119. The audit will be completed by an independent safety auditor.

The proposed access arrangement removes traffic entering the site from Uxbridge Road and removes the possibility of vehicles queueing on the Uxbridge Road to enter the site, which is considered an improvement compared to the existing situation.

Therefore SBC Highways and Transport have no objection to the proposed vehicular access arrangements for the proposed development.

Pedestrian Access

The existing pedestrian provision on Diamond Road will be retained, with no changes to the footways or pedestrian ramp to Uxbridge Road.

It has been demonstrated that pedestrian visibility splays of 2.4m x 2.4m can be provided from the proposed vehicular access for the site.

Access by Sustainable Travel Modes

The site benefits from a reasonable level of accessibility by sustainable travel modes. The nearest bus stop to the site is the northbound bus stop on the Uxbridge Road which is approximately 70m from the site. Slough Railway Station is approximately 1100m (14 minutes' walk) from the site.

Trip Generation and Traffic Impact

The Transport Consultant (Cotswold Transport Planning) has provided a calculation of the trip generation for the site's previous use as a hand car wash to understand the net impact of the site's redevelopment on trip generation. The calculation is based on survey data of similar hand car wash sites from TRICS, the national trip generation database. It is estimated that the existing hand car wash generated 7 two-way vehicle trips during the AM Peak Hour and 15 two-way trips during the PM Peak Hour and 244 two-way trips during a 12-hour day (0700 –

1900).

The Transport Statement states that the proposed building will be occupied by 40 users of the teaching facility. It is assumed that 25% of users will travel by non-car modes, which would equate to 10 trips, while the remaining 30 users would travel by car, with 50% of users from the same families car sharing.

As a result, the TS concludes the site will generate 15 vehicle trips on weekday evenings when teaching will take place. This is based on 40 users during the evening and 25% of users travelling by non-car modes. The TS details that the proposed space could accommodate upto 80 users and that even if the building were 100% occupied, this would equate to 30 one-way vehicle trips during an evening of teaching. The facility is expected to generate fewer vehicle trips than the estimated 244 two-way vehicle trips generated by the existing hand car wash.

SBC Highways and Transport have no objection to the proposed development due to the site's vehicular trip generation or due to it's impact on highway capacity.

Layout

At the request of SBC Highways and Transport it has been confirmed that the car park will have a minimum overhead clearance of 2.25m throughout the car park and a maximum ramp gradient of 1:13.

At the request of Highways and Transport, the transport consultant for the proposed development has provided swept path analysis using a large car measuring 5.1m long on Drawing CTP-20-467 SK01 and SK02 Rev D which demonstrate that a large car can ingress and egress the end parking spaces.

All car parking spaces bounded by a wall have been widened by 300mm to measure 2.7m x 4.8m to ensure space for car doors to be opened where they cannot be opened over an adjacent space.

SBC Highways and Transport have no objection to the parking layout for the proposed development.

Car Parking

A total of 48 parking spaces are proposed to support the proposed development, with 24 spaces on the ground floor of the proposed car park and 24 parking spaces on the upper deck of the proposed car park. 3 of the proposed spaces will be designed to an accessible standard. The Mosque benefits from 25 existing parking spaces located behind the mosque and accessed from Diamond Road.

The proposed parking will offer overflow parking for the Diamond Road Mosque during peak periods of usage. Given the teaching activities already take place on site, the proposed facility and proposed parking are expected to relieve the existing parking pressures on Diamond Road.

Cycle Parking

8 cycle stands will be provided providing parking for 16 cycles. SBC accept the proposed cycle parking provision.

Servicing and Refuse Collection

At the request of SBC Highways and Transport, it has been confirmed that the bins would be sited on the lower level and there would be a maximum gradient of 1:40 in between the bin store and the road. SBC require the applicant to clarify the gradients between the bin store and kerbside collection point.

Summary and Conclusions

Subject to the applicant providing the requested information to allay my concerns, I confirm that I have no objection to this application from a transport and highway perspective.

6.2 Thames Water

Waste Comments

With the information provided, Thames Water has been unable to determine the waste water infrastructure needs of this application. Thames Water has contacted the developer in an attempt to obtain this information and agree a position for FOUL WATER drainage, but have been unable to do so in the time available and as such, Thames Water request that the following condition be added to any planning permission. "No development shall be occupied until confirmation has been provided that either:- 1. Capacity exists off site to serve the development, or 2. A development and infrastructure phasing plan has been agreed with the Local Authority in consultation with Thames Water. Where a development and infrastructure phasing plan is agreed, no occupation shall take place other than in accordance with the agreed development and infrastructure phasing plan, or 3. All wastewater network upgrades required to accommodate the additional flows from the development have been completed. Reason - Network reinforcement works may be required to accommodate the proposed development. Any reinforcement works identified will be necessary in order to avoid sewage flooding and/or potential pollution incidents. The developer can request information to support the discharge of this condition by visiting the Thames Water website at thameswater.co.uk/preplanning. Should the Local Planning Authority consider the above recommendation inappropriate or are unable to include it in the decision notice, it is important that the Local Planning Authority liaises with Thames Water Development Planning Department (telephone 0203 577 9998) prior to the planning application approval.

Water Comments

If you are planning on using mains water for construction purposes, it's important you let Thames Water know before you start using it, to avoid potential fines for improper usage. More information and how to apply can be found online at thameswater.co.uk/buildingwater.

The proposed development is located within 15m of our underground water assets and as such we would like the following informative attached to any approval granted. The proposed development is located within 15m of Thames Waters underground assets, as such the development could cause the assets to fail if appropriate measures are not taken. Please read our guide 'working near our assets' to ensure your workings are in line with the necessary processes you need to follow if you're considering working above or near our pipes or other structures. <https://developers.thameswater.co.uk/Developing-a-large-site/Planning-your-development/Working-near-or-diverting-our-pipes>. Should you require further information please contact Thames Water. Email: developer.services@thameswater.co.uk

On the basis of information provided, Thames Water would advise that with regard to water network and water treatment infrastructure capacity, we would not have any objection to the above planning application. Thames Water recommends the following informative be attached to this planning permission. Thames Water will aim to provide customers with a minimum pressure of 10m head (approx 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.

Supplementary Comments

We require confirmation on where the Foul Water discharge will connect to the network, and whether it will connect via gravity or pumped. Also, confirmation on whether Surface Water is intended to enter the public network and, if so, at what discharge rate?

6.3 Contamination officer

No comments received to date.

6.4 Environment Agency

We regret that Thames Area Sustainable Places is unable to provide a detailed response to this application at this time. We are currently only providing bespoke responses to the highest risk cases. The advice below constitutes our substantive response to the consultation under the terms of the Development Management Procedure Order 2015.

We have checked the environmental constraints for the location and have the following guidance:

The proposal is for/includes a learning and non-residential institution, for, or in connection with, public worship or religious instruction and the environmental risks in this area relate to :

- Groundwater Protection
- Other consents

Groundwater Protection

If infiltration drainage is proposed then it must be demonstrated that it will not pose a risk to groundwater quality. We consider any infiltration SuDS greater than 3m below ground level to be a deep system and generally not acceptable. All infiltration SuDS require a minimum of 1m clearance between the base of the infiltration point and the peak seasonal groundwater levels. All need to meet the criteria set out in our Groundwater Protection publication. In addition, they must not be constructed in ground affected by contamination.

Other Consents

As you are aware we also have a regulatory role in issuing legally required consents, permits or licences for various activities. We have not assessed whether consent will be required under our regulatory role and therefore this letter does not indicate that permission will be given by the Environment Agency as a regulatory body.

6.5 Lead Local Flood Authority

We have reviewed the following information in relation to the above planning application:

- DRAINAGE DESIGN Version 1.0 RAB: 2689B23/03/2021

The submitted information addresses our requirements and we have no further comments.

7.0 **Policy Background**

7.1 Slough Local Development Framework Core Strategy 2006-2026 Development Plan Document policies:

- Core Policy 1 – Spatial Strategy
- Core Policy 6 – Retail Leisure and Community Facilities
- Core Policy 7 – Transport
- Core Policy 8 – Sustainability and the Environment
- Core Policy 9 – Natural and Built Environment
- Core Policy 10 – Infrastructure
- Core Policy 11 – Social Cohesiveness
- Core Policy 12 – Community Safety

Local Plan for Slough March 2004 policies:

- EN1 – Standard of Design
- EN3 – Landscaping Requirements
- EN5 – Design and Crime Prevention
- T2 - Parking Restraint
- T8 - Cycling Network and Facilities

Slough Local Development Plan and the NPPF

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that applications for planning permission are determined in accordance with the development plan unless material considerations indicate otherwise. Annex 1 to the National Planning Policy Framework advises that due weight should be given to relevant policies in existing plans according to their degree of consistency with the Framework (the closer the policies in the plan to the policies in the Framework, the greater the weight that may be given).

The revised version of the National Planning Policy Framework (NPPF) was published in July 2021. Planning Officers have considered the proposed development against the revised NPPF which has been used together with other material planning considerations to assess this planning application.

The NPPF states that decision-makers at every level should seek to approve applications for sustainable development where possible and planning law requires that applications for planning permission be determined in accordance with the development plan, unless material considerations indicate otherwise.

7.2 Slough Local Development Plan and the NPPF

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8.0 Planning Considerations

8.1 The planning considerations for this proposal are:

- Principle of the proposal
- Design and impact on the character and appearance of the area
- Impact on neighbouring amenity
- Highways and transport
- Contamination
- Trees and ecology
- Drainage Considerations

9.0 Principle of Development

9.1 The revised NPPF, in general terms, supports the redevelopment of previously developed land and states, in para 119, that planning decisions should promote and effective use of land in meeting the need for homes and other uses.

9.2 Core Policy 6 of the Core Strategy only addresses community facilities in respect of their retention rather than provision. The pre-amble to the policy states that the provision of new community facilities to meet local needs will be encouraged.

9.3 The application site is in a relatively central location and has been vacant for a relatively long period. Its previous use as a car wash was granted temporary consent but was not considered acceptable in planning terms to be granted permanently.

9.4 Objection was received over the extent of the community that would benefit from the proposal. As stated above the applicant has advised that the proposal is required to re-provide the education aspect of the religion. Management of the building would be with the applicant and it would appear that the facility would be used by the Islamic Trust community rather than the wider community. In planning terms there is no requirement that a community facility is available for all, and likewise, there is nothing to say that a community proposal that is for the benefit of a particular group is unacceptable.

9.5 The redevelopment proposal in this application will result in a permanent use for the site and is one that, in principle terms, can be supported by planning policies. However the overall principle of development is dependent on the detailed considerations to come in this report.

10.0 Design and Impact on Appearance and Character of the area

10.1 Policy EN1 of the Local Plan outlines that development proposals are required to reflect a high standard of design and must be compatible with and/or improve their surroundings in terms of scale, height, massing, layout, siting, building form and design, architectural style, materials, access points, visual impact, relationship to nearby properties, relationship to mature trees, and relationship to water course. Poor designs which are not in keeping with their surroundings and schemes that overdevelop the site will not be permitted.

10.2 Core Strategy Policy 8 states that all development in the borough shall be sustainable, of a high quality design, improve the quality of the environment and address the impact of climate change. Core Policy 8 outlines:

'All development will:

- a) Be of a high quality design that is practical, attractive, safe, accessible and adaptable;*
- b) Respect its location and surroundings;*
- c) Provide appropriate public space, amenity space and landscaping as an integral part of the design; and*
- d) Be in accordance with the Spatial Strategy in terms of its height, scale, massing and architectural style.'*

10.3 The building is proposed as a 3 storey structure when viewed from Uxbridge Road. The design from this perspective shows a balanced design with a commercial/civic character to it in respect of window spacing and detailing. From the rear the building reads as a four storey structure to accommodate the land levels although the access ramp screens the lower floor parking area. The design of the building proposes windows on the site elevations ensuring there are no blank elevations and the design incorporates false windows where there may be amenity impacts to provide visual interest.

10.4 The scale of the building is higher than the general scale of dwellings to the west of the site however it has a comparable scale to the existing building to the southeast, on the other side of Uxbridge Road, this building houses the Premier Inn and is 4 storeys in height which would make it higher than this scheme when viewed from Uxbridge Road. The form of the proposal is also comparable to this adjacent building and means that it should sit comfortably in the street scene. The propose building would also sit comfortably next to the existing buildings on the same side of Uxbridge Road. It is noted that these are two storey but the varying scales means that the proposal will not be out of character.

10.5 The proposal will sit higher than the existing dwellings to the west of the site as there are predominantly two storey. It would have a visual relationship with the dwellings on Diamond Road however this would be limited to the southeastern end of the road at which point the character transitions away from terraced residential to a more open and mixed character. The building would also be visible

from the public realm areas of Connaught Road but, again, this would be at a point where the character is mixed and other 3 storey buildings are prominent. As a result the building would not look out of character.

10.6 In principle the proposed materials palette is considered acceptable, approval of specific details is proposed to be secured by condition. The redevelopment of the site will amount to an enhancement of the built form in this area. The site has, to date, had a temporary character to it and the redevelopment will establish permanent built form which will improve the urban character of the area.

10.6 On the basis of the considerations above, it is considered that the proposed development will accord with policies EN1 of the Local Plan and CP8 of the Core Strategy and the requirements of the NPPF 2019.

11.0 Impact on neighbouring amenity

11.1 Policy CP8 of the Core Strategy requires that the design of all new development should respect the amenities of adjoining occupiers. The proposed development does not include any extensions to the building or any additional openings. The replacement of external cladding will have no harmful impact on the amenities of neighbouring occupiers and the proposed development is therefore considered to comply with policy CP8 of the Core Strategy and the requirements of the NPPF 2019.

11.2 The building is a 3/4 storey structure and will increase the prominence of built form on the site. The existing residential community is located to the northwest of the application site.

11.3 Objection has been received on the grounds of overlooking and loss for privacy to specifically, 1 India Road and more generally to the wider area. The layout of the proposal is such that there will be windows on the second and third floor that face northeast, towards residential units on Diamond Road, namely 59-69 Diamond Road. The nature of the proposed use is such that there would not be the type of perceived impact that would be associated with a residential use however the arrangement of windows are such that there would be a perception of overlooking to the immediate neighbours. It is noted that a landscape screen is proposed on the common boundary with these properties but the scale of the building is such that it is unlikely to provide a suitable screen that would remove overlooking impacts.

11.4 It is considered that the concerns raised above can be addressed through requiring the relevant windows to be installed with obscure glazing which would remove the ability to overlook to the rear.

11.5 Taking account of the adjacent land uses it is considered that there would not be any overlooking impacts from the front elevation or either side elevation. Having regard to the location of the site and its relationship with 1 India Road, it is considered that this property is not immediately adjacent to the site and is at such a distance from the site that there would be no direct overlooking impact that would cause a significant adverse impact.

11.6 The siting of the proposed building is such that there is not considered to be any overbearing impact on neighboring residents due to offset nature of dwelling layouts and the distances to these sites.

- 11.7 Objections are received on the grounds of increased noise impacts from the use. As stated above the proposed development is required to accommodate the existing users of the mosque and as a result there is no concern over noise impacts from any increased number of people. In respect of noise from the existing use, the application cannot be used to address pre-existing matters on the other site. Therefore consideration is limited to noise impact resulting from activities associated with the proposed building itself. The proposed uses as outlined in the report are not the type that would be associated with excessive noise impacts and therefore there are no concerns with this impact as a result.
- 11.8 Related to this point, objection was also raised over the lack of clarity over the opening hours from the proposal. It is considered that this is a justified concern and with the building being located at the edge of a residential area, inappropriate opening hours can potentially have an adverse impact on amenity. As a result it is considered reasonable to limit operating hours in the interests of neighbouring amenity and the recommendation includes a condition that limits operating hours to between 0800-2200 daily.
- 11.9 As a result of the above assessment, the proposal is considered to be acceptable in light of Core Policy 8 of the Local Development Framework Core Strategy and Policies EN1 and EN2 of the Adopted Local Plan.

12.0 Highways and Parking

- 12.1 The National Planning Policy Framework states that planning should seek to development is located where the need to travel will be minimised and the use of sustainable transport modes can be maximised. Development should be located and designed where practical to create safe and secure layouts which minimise conflicts between traffic and pedestrians. Where appropriate local parking standards should be applied to secure appropriate levels of parking. This is reflected in Core Policy 7 and Local Plan Policies T2 and T8. Paragraph 32 of the National Planning Policy Framework states that 'Development should only be prevented or refused on transport grounds where the residual cumulative impacts of development are severe'.
- 12.2 The application process has required the applicant to provide additional information in relation to highways impacts. The initial concerns related to the use of the existing access onto Uxbridge Road as part of the scheme as there were concerns with vehicle speeds on this road and visibility achieved at the access for people leaving the site. Following the submission of the additional information it was confirmed that there were no highways objections to the scheme.
- 12.3 In respect of parking the scheme provides 46 parking spaces which is considered to be acceptable. Significantly the parking provision will be able to be used by worshippers at the mosque which should ease parking pressures on the surrounding residential streets. The objections on parking grounds are noted but as the proposal is a building to accommodate existing users of the mosque there is no significant increase in vehicle numbers associated with the mosque there should be a reduction in any parking issues on these streets and therefore the scheme benefits the area in this respect.
- 12.4 There are also objections received on the grounds of increased congestion. The redevelopment of the site will open it up and bring an existing egress point onto

Uxbridge road back into use. This allows for users of the mosque and proposed building to leave the area via Uxbridge road and therefore this would serve to ease any congestion on the surrounding neighbouring streets. Also it will not be possible to enter the site from Uxbridge Road which ensures the reopening of this access can be done safely.

12.5 Cycle parking provision is provided in the lower ground floor area which would be accessible and secure.

12.6 Objection was received in respect of how the waste management operations would take place at the site. No details are provided with this application although it is noted the site is in an accessible location and there would be no concerns over waste collection in principle. A condition is included as part of the recommendation to require management details to be approved.

12.7 On the basis of the considerations made above, there are no highways impacts that would result in significant and demonstrable harm and the proposal has the potential to improve circumstances in the area. As a result there are no objections in highway terms.

13.0 Drainage

13.1 The site is located within flood zone 1 and therefore flood risk is minimal. The site is previously developed and was last used as a car wash which would have subjected it to significant levels of surface water.

13.2 There has been no objection from Thames Water or the Environment Agency over the scheme, The Lead Local Flood Authority has asked for a set of information to determine the impact on flood risk and after the provision of this it was confirmed that there is no objection on drainage grounds. The recommendation will include the conditions requested by Thames Water and ensure drainage proposals are implemented as proposed.

14.0 Contamination

14.1 Core Policy 8 (Sustainability and the Environment) of the SBC's Core Strategy Document states that development shall not 'cause contamination or deterioration in land, soil or water quality' nor shall development occur on polluted land unless appropriate mitigation measures are employed.

14.2 At the time of drafting this report no comments have been received and Members will be updated.

15.0 Landscape and Ecology.

15.1 Paragraph 17 of the NPPF 2019 states that when determining planning applications, if significant harm to biodiversity cannot be avoided or adequately mitigated or as a last resort compensated for then planning permission should be refused. It also states that opportunities to incorporate biodiversity improvements in and around the developments should be encouraged, especially where this can secure measurable net gains for biodiversity.

15.2 The site is currently devoid of natural features outside of weeds and the proposed development includes a landscape strip on the northwestern boundary. While the

principle purpose of this is to provide a screen from neighbouring properties, it will also result in the provision of biodiversity enhancements on the site, albeit on a small scale. It would however result in a net gain in biodiversity which is acceptable in planning terms.

- 15.3 The detail of the landscaping scheme can be approved by condition and therefore as a result there are no objections in respect of landscaping and ecology impacts.

16.0 Process

- 16.1 It is the view of the Local Planning Authority that the proposed development does improve the economic, social and environmental conditions of the area for the reasons given in this notice and it is in accordance with the National Planning Policy Framework.

17.0 PART C: RECOMMENDATION

- 17.1 Having considered the relevant policies set out above, comments from consultees and neighbours representations as well as all relevant material considerations it is recommended the application be **approved** subject to the following conditions.

18.0 PART D: CONDITIONS

The following conditions are recommended.

1. The development hereby permitted shall be commenced within three years from the date of this permission.

REASON To prevent the accumulation of planning permissions, and to enable the Council to review the suitability of the development in the light of altered circumstances and to comply with the provisions of Section 91 of the Town and Country Planning Act 1990.

2. The development hereby approved shall be implemented only in accordance with the following plans and drawings hereby approved by the Local Planning Authority:

- (a) Drawing No. P10 Rev A, undated, Recd On 22/09/2021
- (b) Drawing No. P8 Rev A, undated, Recd On 16/09/2021
- (c) Drawing No. P7, undated, Recd On 11/01/2021
- (d) Drawing No. P9, undated, Recd On 11/01/2021
- (e) DRAINAGE DESIGN Version 1.0 RAB: 2689B, dated 23/03/2021

REASON To ensure that the site is developed in accordance with the submitted application and to ensure that the proposed development does not prejudice the amenity of the area and to comply with the policies in The Local Plan for Slough 2004.

3. No development shall take place until details have been submitted to and approved in writing by the Local Planning Authority of the proposed external facing materials on the dwellings hereby approved. The development shall be carried out in accordance with the details approved unless otherwise agreed in writing with the Local Planning Authority.

REASON To ensure a satisfactory appearance of the development so as not to prejudice the visual amenity of the locality in accordance with Policy EN1 of The Adopted Local Plan for Slough 2004.

4. The windows on the rear northwest facing elevation at first and second floor levels of the development hereby approved shall be glazed with obscure glass in accordance with a sample to be first submitted to and approved in writing by the Local Planning Authority and any of these that are opening windows shall be at a high level (above 1.8m) only, and shall be so maintained unless prior written approval has been obtained from the Local Planning Authority.

REASON To protect the privacy of adjoining occupiers.

5. No window(s), other than those hereby approved, shall be formed in the flank or rear elevations of the development without the prior written approval of the Local Planning Authority.

REASON To minimise any loss of privacy to occupiers of adjoining residential properties.

6. The uses hereby approved shall not commence until a scheme has been submitted to and approved in writing by the Local Planning Authority for external site lighting including details of the lighting units, levels of illumination and hours of use. The approved details shall be implemented prior to any uses commencing and no lighting shall be provided at the site other than in accordance with the approved scheme.

REASON In the interests of the neighbouring property.

7. No development shall commence on site until details of the proposed boundary treatment including position, external appearance, height and materials have been submitted to and approved by the Local Planning Authority. Before the development hereby permitted is occupied, a suitable means of his boundary treatment shall be implemented on site prior to the first occupation of the development and retained at all time on the future.

REASON In the interests of the visual amenity of the area.

8. No development shall commence on site until a detailed landscaping and tree planting scheme has been submitted to and approved in writing by the Local Planning Authority. This scheme should include the type, density, position and planting heights of new trees and shrubs along the rear boundary.

The approved scheme shall be carried out no later than the first planting season following completion of the development. Within a five year period following the implementation of the scheme, if any of the new or retained trees or shrubs should die, are removed or become seriously damaged or diseased, then they shall be replaced in the next planting season with another of the same species and size as agreed in the landscaping tree planting scheme by the Local Planning Authority.

REASON To protect the amenity of residents to the rear of the site and in the interests of the visual amenity of the area.

9. During the demolition / construction phase of the development hereby

permitted, no work shall be carried out on the site outside the hours of 0800 hours to 1800 hours Mondays - Fridays, 0800 hours - 1300 hours on Saturdays and at no time on Sundays and Bank/Public Holidays.

REASON To protect the amenity of residents within the vicinity of the site.

10. No development shall be occupied until confirmation has been provided that either:-

1. Capacity exists off site to serve the development, or
2. A development and infrastructure phasing plan has been agreed with the Local Authority in consultation with Thames Water. Where a development and infrastructure phasing plan is agreed, no occupation shall take place other than in accordance with the agreed development and infrastructure phasing plan, or
3. All wastewater network upgrades required to accommodate the additional flows from the development have been completed.

Reason - Network reinforcement works may be required to accommodate the proposed development.

11. The use hereby permitted shall not be open to members of the public / customers outside the hours of 08:00 hours to 22:00 hours on Mondays- Thursdays, 08:00 hours to 22:00 hours on Fridays and Saturdays, and 08:00 hours to 22:00 hours on Sundays and Bank/Public Holidays.

REASON To protect the amenity of residents within the vicinity of the site in accordance with Policy EN26 of The Adopted Local Plan for Slough 2004.

12. The development shall not commence until the new means of access has been constructed in accordance with the approved drawing and constructed in accordance with Slough Borough Council's Design Guide.

REASON: In order to minimise danger, obstruction and inconvenience to users of the highway and of the development.

13. No part of the development shall be occupied until the redundant means of access has been removed and the footway re-instated and laid out in accordance with the plans to be submitted to and approved in writing by the local planning authority and the works constructed in accordance with Slough Borough Council's Design Guide.

REASON: In order to minimise danger, obstruction and inconvenience to users of the highway and of the development.

14. The scheme for parking and manoeuvring indicated on the submitted plans shall be laid out prior to the initial occupation of the development hereby permitted and that area shall not thereafter be used for any other purpose.

REASON: To enable vehicles to draw off, park and turn clear of the highway to minimise danger, obstruction and inconvenience to users of the adjoining highway.

15. Prior to the development hereby approved first being brought into use, 48 no. car parking spaces shall be provided and made available for use in connection

with the religious use and maintained for the parking of cars thereafter. The car parking spaces shall not be used for any separate business, commercial, or residential use.

REASON: In the interests of ensuring that the use benefits from satisfactory car parking provision in the interests of the amenities of the area in accordance with Core Policy 7 of the Slough Local Development Framework, Core Strategy 2006-2026, Development Plan Document, December 2008'.

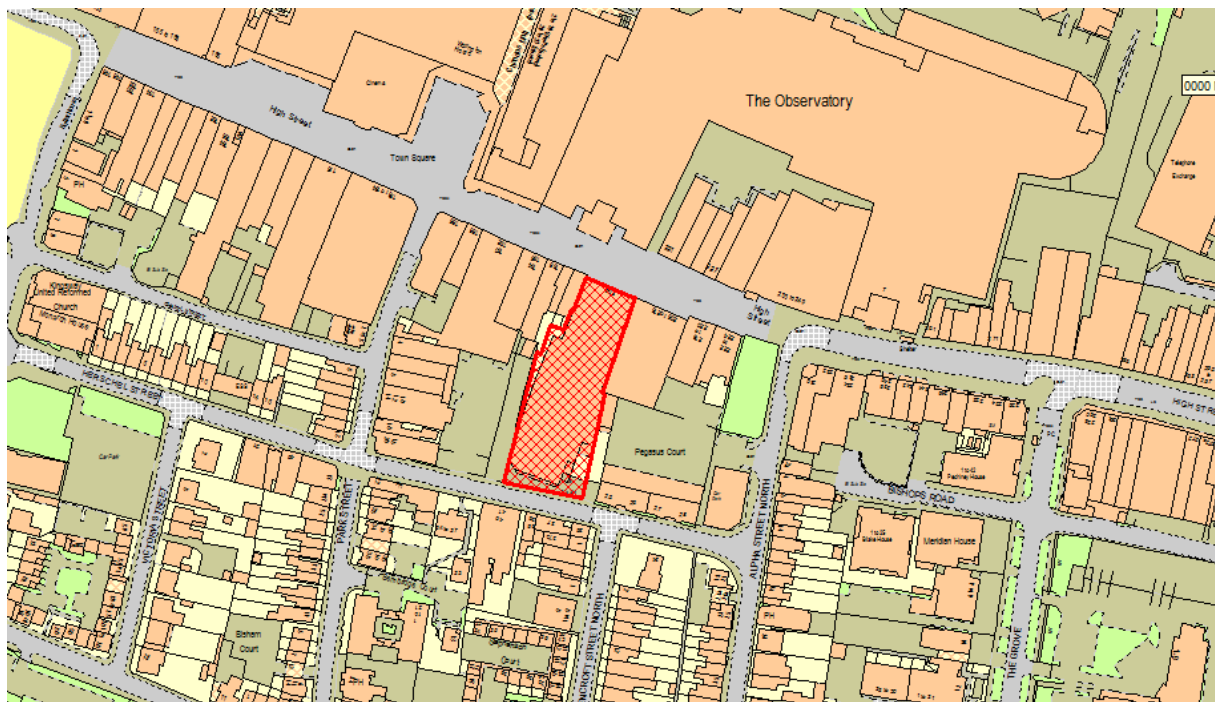
16. No other part of the development shall be occupied until the visibility splays shown on the approved drawings have been provided on both sides of the access and the area contained within the splays shall be kept free of any obstruction exceeding 600 mm in height above the nearside channel level of the carriageway.

REASON: To provide adequate intervisibility between the access and the existing public highway for the safety and convenience of users of the highway and of the access.

PLANNING APPLICATION REPORT

App No	P/02683/015	Applicant	WMC (Slough) Ltd
		Agent	Mr. Chris Brown, Rolfe Judd Planning
Received	3rd August 2021		Old Church Court, Claylands Road, London, SW8 1NZ
Officer	Nyra John		
Level	Delegated	App Type	Variation of Conditions (Major)
Ward	Central	13 Week Date	2 nd November 2021
Location	204-206, High Street, Slough, SL1 1JS		
Proposal	<p>An application under Section 73 of the Town and Country Planning Act 1990 (as amended) to vary Condition 2 (Approved Drawing Numbers) , Condition 5 (Approved Uses) and Condition 6 (Hours of Operation) of planning permission P/02683/013 for the Demolition and Redevelopment of the existing site for a mixed use development (granted 4th March 2020) namely for various material amendments including addition of 2 'floors' to the top of Block B to create an 11th and 12th floor and an additional 8 x 1 bed flats; use of the first floor of Block A for 3 x 2 bed residential units; flexible Class E space throughout the ground floor commercial unit; amendment to the commercial High St side entrance to form a residential entrance; increasing the number or cycle parking spaces and waste provision; and removal of the flexible office/gym space at first floor of Block A</p>		

Recommendation: Delegate to the Planning Manager for Approval.



1 SUMMARY OF RECOMMENDATION

1.1 Having considered the relevant policies of the Development Plan set out below, the representations received from consultees and the community along with all relevant material considerations, it is recommended the application be delegated to the Planning Manager for:.

A. Approval subject to:

(i) the satisfactory completion of a Section 106 Deed of Variation to secure additional financial contributions towards education improvements and open space/recreation improvements in the local area; and

(ii) finalising conditions and any other minor changes;

B. Refuse the application if the completion of the Section 106 Agreement is not finalised by 31st March 2022 unless a longer period is agreed by the Planning Manager in consultation with the Chair of the Planning Committee;

1.2 The original consented application was previously presented to the planning committee October 2019 where members delegated to the Planning Manager for approval.

1.3 This application has subsequently been amended by the applicant and has submitted amended plans and technical reports. It is being brought to Committee for decision, at the request of the Planning Manager and because it remains a major development within Slough Town Centre.

PART A: BACKGROUND

2 Site and Surroundings

2.1 The 0.24ha site comprises the vacant former BHS Store at 204-206 High Street, in Slough. The site consists of the building and areas of hardstanding used for associated parking and servicing. The existing building comprises 3 commercial storeys in height fronting onto the High Street and is currently formed of a hoarded up shop front at ground floor, with a two storey tiled facade above. The height of the building rises to 4 storeys to its rear on Herschel Street. The flank walls around the eastern site boundary for much of its length.

2.2 The site's access points and presence on the High Street create a linear retail unit from north to south. The existing building has windows on the flank walls overlooking the adjacent properties to the East and West of the site. The Herschel Street facade is blank with only an emergency escape door/service entrance on the ground floor.

- 2.3 The site is located within the established Town Centre of Slough on the southern edge of the High Street. The surrounding urban townscape is characterised by buildings of varying style, age and size, transport infrastructure and public realm. The land uses are predominantly retail and commercial to the north of the site and residential to the south of the site.
- 2.4 Historically the High Street has been characterised by mainly 3-4 storey buildings. In the latter half of the 20th century a number of new modern additions were constructed on the High Street, including Queensmere Shopping Centre, a part 8 storey building, constructed in the 1970's. The Shopping Centre and pedestrianised High Street are located immediately north of the site.
- 2.5 The buildings to the east are made up of commercial/retail uses toward the High Street and residential uses toward Herschel Street. There is an open parking area of land to the rear of 210 – 216 High St and Pegasus Court, Herschel St.
- 2.6 To the west are commercial/retail buildings and a larger area of car parking at the rear of 190 – 202 High Street and properties on Park Street.
- 2.7 To the south is Herschel Street, which provides servicing areas for the commercial units fronting the High Street, but also has residential, commercial and community buildings on the southern side. There is no on-street parking. There are potential plans to expand the width of the road on the southern side; however no firm policy has been adopted to date. The mass, form and spacing of buildings fronting the north and south sides of Herschel Street are varied with low rise single storeys to larger modern developments (up to 8 storeys). The area to the south can be characterised as a transitional area between commercial/community and residential uses and scales of premises.
- 2.8 The site is not within a Conservation Area, nor does it contain any statutory listed buildings, but it is in close proximity to two locally listed properties. The Locally Listed Nos. 194-198 and 200-202 High Street are situated to the immediate west of the site located at the High Street frontage. No. 200-202 comprises a three-storey red brick building that adjoins part of the site's western boundary. 208-212 High Street (to the east) comprises a three storey commercial building with ground floor retail and ancillary uses on the upper floors which are set back from the frontage.
- 2.9 Due to the site's Town Centre location, it is well situated to benefit from the majority of the local amenities that Slough has to offer. The area is well served by Great Western Railway trains that run through Slough Station, located around 5 minutes walk from the site. The train service runs west towards Reading and Windsor stations, as well as east towards Southall and London Paddington stations and is expected to be served by Crossrail with a direct route to Central London in the future.

3 Planning History

- 3.1 The original application P/02683/013 was approved with Conditions; Informatives on 4th March 2020. The description of development is as follows:

Demolition and Redevelopment of the existing site for a mixed use development comprising replacement flexible retail space (Class A1,A2,A3 uses) at ground floor level, flexible commercial floorspace at first floor fronting the High Street for either B1 (offices) or Class D2 (gym) uses and 78 residential dwellings within 3 buildings at podium level across the site with heights of 5, 11 and 4 storeys. Shared amenity space provided at first floor podium level, with cycle, waste and recycling storage facilities at ground floor level, and provision of two accessible car parking spaces (for the residential uses), loading and drop-off facilities and servicing area within ground floor level with access from Herschel Street (Revised Description of Development and Revised Plans submitted 03/09/2019)

- 3.2 A non-material amendment (P/02683/014) to planning application P/02683/013 dated 4th March 2020 was approved with Conditions on 9th August 2021 as follows:

Non material amendment to planning permission P/02683/013 dated 04/03/2019 to change to the description of development to remove references to the specific building heights, unit numbers, and floorspace areas and variation of the wording in Condition 2 (Approved Drawings) to include reference to the unit numbers and floorspace areas. The new description of development comprises:

Demolition and Redevelopment of the existing site for a mixed use development comprising replacement flexible retail space fronting the High Street and residential dwellings within 3 buildings at podium level across the site. Shared amenity space provided at first floor podium level, with cycle, waste and recycling storage facilities at ground floor level, and provision of accessible car parking spaces (for the residential uses), loading and drop-off facilities and servicing area within ground floor level with access from Herschel Street

- 3.3 The non-material amendment P/02683/015 was to facilitate the planning permission to remove any reference to specific building height, unit numbers and floorspace areas. It is not possible to amend the description of development through a s73 application (which can only vary planning conditions). Therefore, the s96A application is required to amend the wider development description and refer to specifics in Condition 2 accordingly, in order to facilitate the approval of this concurrent s73 Application (subject to its acceptability in planning terms).

4 The Proposal

- 4.1 The application is made under Section 73 of the Town and Country Planning Act 1990 (as amended) to vary Condition 2 (Approved

Drawing Numbers), Condition 5 (Approved Uses) and Condition 6 (Hours of Operation) of planning permission P/02683/013 for the Demolition and Redevelopment of the existing site for a mixed use development (granted 4th March 2020) namely for various material amendments including:

- addition of 2 'floors' to the top of Block B to create an 11th and 12th floor and an additional 8 x 1 bed flats;
- use of the first floor of Block A for 3 x 2 bed residential units and resulting changes to the front façade including balconies introduced on the first floor facing the High Street;
- flexible Class E space throughout the ground floor commercial unit;
- amendment to the commercial High St side entrance to form a residential entrance and a staircase leading to the High St and lift removed;
- increasing the number of cycle parking spaces for the increase of 11 units and increase and reconfiguration of waste provision to the rear for the increase of 11 units; and
- removal of the flexible office/ gym space at first floor of Block A.

4.2 The scheme proposes a total of 51 no. 1 bed units (57%) and 38 no. 2 bed units (43%). 6 no. (7%) of the flats are wheelchair accessible units.

4.3 766sqm of private amenity space is provided in the form of balconies and private gardens. There is proposed 1169sqm of communal amenity space, comprising 233sqm at ground floor level, 788sqm across two landscape areas on the first floor podium level and 2no 148sqm roof terraces on upper floors of building B.

4.4 Condition 2 of permission P/02683/013 lists the following drawings relating to the application:

Condition 2. Approved plans

The development hereby approved shall be implemented only in accordance with the following plans and drawings hereby approved by the Local Planning Authority

Ground Floor Plan A3 P100 S2-P8

First Floor Plan A3 P101 S2-P5

Second Floor Plan A3 P102 S2-P5

Third Floor Plan A3 P103 S2-P5

Fourth Floor Plan A3 P104 S2-P6

Fifth Floor Plan A3 P105 S2-P6

Sixth Floor Plan A3 P106 S2-P5

Seventh Floor Plan A3 P107 S2-P5

Eighth Floor Plan A7 P108 S2-P6

Ninth Floor Plan A3 P109 S2-P6

Tenth Floor Plan A3 P110 S2-P6

Roof Plan A3 P114 S2-P6

High Street Elevation A2 P200 S2-P4

Herschel Street Elevation A2 P201 S2-P5

Section AA, BB & CC A2 P300 S2-P5

Section DD & EE A2 P301 S2-P5
Section FF A2 P302 S2-P5
Section GG A2 P303 S2-P4
Section HH A2 P304 S2-P5

REASON: For the avoidance of doubt, to ensure that the site is developed in accordance with the submitted application and to ensure that the proposed development does not prejudice the amenity of the area to comply Policy EN1 of The Local Adopted Plan for Slough 2004, Core Policy 9 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2019).

This S73 application seeks to remove the above approved drawings and replace with the following proposed drawings:

Condition 2. Approved plans

The development hereby approved shall be implemented only in accordance with the following plans and drawings hereby approved by the Local Planning Authority

Ground Floor Plan HSL P100 S2-P12
First Floor Plan HSL P101 S2-P8
Second Floor Plan HSL P102 S2-P6
Third Floor Plan HSL P103 S2-P6
Fourth Floor Plan HSL P104 S2-P7
Fifth Floor Plan HSL P105 S2-P7
Sixth Floor Plan HSL P106 S2-P6
Seventh Floor Plan HSL P107 S2-P6
Eighth Floor Plan HSL P108 S2-P8
Ninth Floor Plan HSL P109 S2-P8
Tenth Floor Plan HSL P110 S2-P8
Roof Plan AHSL P114 S2-P9
High Street Elevation HSL P200 S2-P7
Herschel Street Elevation HSL P201 S2-P7
Section AA, BB & CC HSL P300 S2-P6
Section DD & EE HSL P301 S2-P6
Section FF HSL P302 S2-P6
Section GG HSL P303 S2-P5
Section HH HSL P304 S2-P8

REASON: For the avoidance of doubt, to ensure that the site is developed in accordance with the submitted application and to ensure that the proposed development does not prejudice the amenity of the area to comply Policy EN1 of The Local Adopted Plan for Slough 2004, Core Policy 9 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2021).

4.5 The S73 application is submitted with the following additional documents, which accompany the application:

- Planning Statement – prepared by Rolfe Judd, dated 26/07/2021
- Design, Access and Heritage Statement – prepared by Stephen Davy Peter Smith Architects, dated July 2021
- Transport Statement Addendum – prepared by RSK, dated 18/07/2021
- Daylight/Sunlight Report Addendum – prepared by BVP, dated July 2021
- Drainage/SUDS Addendum – prepared by RSK, dated 05/07/2021
- Approved Archaeological Desktop Report – prepared by RSK, dated December 2018
- Ecological Constraints and Preliminary Roost Assessment Report – prepared by RSK, dated 21/07/2021
- Sustainability Statement Addendum – prepared by RSK, dated 21/07/2021
- Energy Statement – prepared by RSK, dated 21/07/2021
- Noise Assessment – Hawkins Environmental, dated 08/07/2021
- Landscaping Scheme Statement of Conformity– prepared by Wilder Associates
- Townscape and Visual Impact Assessment – prepared by Lichfields, dated July 2021
- Approved Contaminated Land Assessment – prepared by RSK, dated December 2018
- Viability Report Addendum– prepared by Turner Morum LLP (sent separately)

4.6 Condition 5 of the original application formalises the uses which could be accommodated within the ground floor commercial space:

Condition 5. Approved Uses

Notwithstanding the information in the approved plans and subject to the provisions within the relevant conditions set out in this planning permission, the following specified land uses are permitted within the specified floorspace:

Ground Floor Level:

Class A1 (Retail)

Class A2 (Financial and Professional Services)

Class A3 (Restaurant)

First Floor Level in Block A:

Class B1 (Office)

Class D2 (Gym)

As set out in Class E, Part 3, schedule 2 of the General Permitted Development Order 2015 (as amended) or the provisions of the relevant Class/Part upon implementation, this permission benefits from a period during which changes of use of the above specified floorspace between uses A1, A2 and A3 (at ground floor level) and B1 or D2 (at First floor level in Block A) as set out in the description of development

may take place without the need for further planning permissions. This flexibility is for a period of ten years from the date of the approval.

Reason: To ensure the uses are compatible with the adjoining land uses and to ensure that the amenity of occupiers residing in surrounding residential properties would be safeguarded in accordance with policies EMP2 and OSC15 of The Adopted Local Plan for Slough 2004, Core Policies 1, 5 and 6 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2019).

- 4.7 Since the scheme was approved, the Use Classes order has been updated on 1st September 2020. Most of the approved uses which are relevant to the scheme have now been placed within a new Class E use in order to increase the flexibility of commercial units. The S73 application seeks to specify which Class E uses could occupy the unit via planning conditions as follows:

Condition 5. Approved Uses

Notwithstanding the information in the approved plans and subject to the provisions within the relevant conditions set out in this planning permission, the following specified land uses are permitted within the specified floorspace:

Notwithstanding the information in the approved plans and subject to the provisions within the relevant conditions set out in this planning permission, the following specified land uses under Class E (Commercial) are permitted within the specified floorspace:

- a) retail, other than hot food;
- b) food and drink;
- c) professional services;
- d) indoor sport, recreation or fitness;
- e) Medical or health services; and
- f) Creche, day nursery or day centre.

As set out in Class E, Part 3, Schedule 2 of the General Permitted Development Order 2015 (as amended) or the provisions of the relevant Class/Part upon implementation, this permission benefits from a period during which changes of use of the above specified floorspace between uses Class E as set out in the description of development may take place without the need for further planning permissions. Permitted development under Schedule 2, Part 3 Class MA (General Permitted Development) (England) Order 2015 to change Class E to Class C3 (residential) are removed. This flexibility is for a period of ten years from the date of the approval.

Reason: To ensure the uses are compatible with the adjoining land uses and to ensure that the amenity of occupiers residing in surrounding residential properties would be safeguarded in accordance

with policies EMP2 and OSC15 of The Adopted Local Plan for Slough 2004, Core Policies 1, 5 and 6 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2021).

- 4.8 Condition 6 outlines the approved hours of use for each use:

Condition 6. Hours of Operation

(A) The Class A1, A2 and A3 uses hereby permitted shall operate only between 0700 hours and 2300 hours, on weekdays and on Saturdays and on 0700 hours to 2230 hours on Sundays and Bank Holidays.

(B) The Class B1 or D2 uses hereby permitted shall operate only between 0700 hours and 2100 hours, on weekdays and on Saturdays and on 1000 hours to 2100 hours on Sundays and Bank Holidays

Reason: To ensure that the amenities of surrounding occupiers are not unduly affected by noise and other disturbance, in accordance with Policy EMP2 and OSC15 of The Adopted Local Plan for Slough 2004, Core Policies 1 and 6 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2019).

- 4.9 The changes to the Use Classes means there is a need to update Condition 6 as follows:

Condition 6. Hours of Operation

(A) The Class E uses hereby permitted shall operate only between 0700 hours and 2300 hours, on weekdays and on Saturdays and on 0700 hours to 2230 hours on Sundays and Bank Holidays.

Reason: To ensure that the amenities of surrounding occupiers are not unduly affected by noise and other disturbance, in accordance with Policy EMP2 and OSC15 of The Adopted Local Plan for Slough 2004, Core Policies 1 and 6 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2021).

5 Consultations

- 5.1 A neighbour notification exercise was undertaken through site notices which were placed on 11th August 2021 at High Street and Herschel Street. A press notice was issued in the Slough Express on 3rd September 2021. No objections were raised between the consultation period (11th August 2021- 1st September 2021).

- 5.2 The following comments have been received following consultation exercises carried out in August and September.

Sustainable Design and Construction

- 5.3 Response received 12th August 2021: I confirm the details submitted (Sustainability Statement dated 21st June 2021 and Energy Statement dated 17th June 2021) are acceptable re current published guidance. I suggest you inform the applicant that the Council is about to change its guidance re implementation of policy and will be seeking greater sustainable design and construction requirements re climate change. Cabinet is due to consider a climate change strategy and action plan but it is not available yet. And Government is about to announce a revised policy on heat and buildings. I suspect that the developer could relatively easily go for at least 20% better than building regulations. 20% is the current limit, mentioned in PPG, that Planning can stipulate re variation from Building Regs Part L.

Local Lead Flooding Authority

- 5.4 Response received 2nd September 2021: We have reviewed the following information in relation to the above planning application: • 680184-R1(1)-SuDS; Sustainable Drainage Strategy, RSK, dated 5th July 2021 Based on the information supplied, the proposed changes will not affect the conditions previously set for foul/surface water drainage (i.e. they can remain outstanding) and we have no additional comments on the proposed changes to conditions 2, 5 and 6.

BPS Chartered Surveyors

- 5.5 A response was received 27th September 2021 through an independent viability review was prepared by BPS on behalf of the Council. The report analysis concludes that the S73 application scheme generates a surplus of £83,300. The report includes updated S106 contributions including for additional open space contributions (£26,700) and education contributions (£229,517) as a result of the additional 11 units. BPS conclude overall that the surplus figure is less than 0.5% GDV and noting minimal changes to the appraisal inputs, this could reduce the surplus further. BPS advises that the scheme's viability may not justifiably support additional affordable dwellings as the surplus is nominal and could change as a result of uncertainties within build costs.

Highways and Transport

- 5.6 A response was received 1st October 2021: I can confirm that there are no significant changes proposed to the application that was approved at the October 2019 planning committee. The most significant changes to the proposal from a Highways and transport perspective are in the following areas;

- Pedestrian Access- improvement from previous application
- Commercial Access- access for commercial unit 3
- Cycle Parking- further details on configuration and layout although number is adequate
- Refuse- further details on refuse strategy
- Servicing- no notable changes

The amended proposal is not vastly different from the approved scheme and therefore we have no objections to this scheme providing that the conditions listed above for the commercial access, design and layout of the cycle storage/parking and the refuse management strategy can be secured.

Environmental Quality

5.7 No comments received.

Landscape

5.8 No comments received.

Thames Water

5.9 No comments received.

Consultation

PART B: PLANNING APPRAISAL

6 Policy Background

6.1 National Planning Policy Framework (2021) and National Planning Policy Guidance: Paragraph 11 of the NPPF states that decisions should apply the presumption in favour of sustainable development which means:

- c) approving development proposals that accord with an up-to-date development plan without delay; or
- d) where there are no relevant development plan policies, or the policies which are most important for determining the application are out-of-date granting permission unless:
 - i. the application of policies in this Framework that protect areas or assets of particular importance provides a clear reason for refusing the development proposed (footnote 6); or
 - ii. any adverse impacts of doing so would significantly and demonstrably outweigh the benefits, when assessed against the policies in this Framework taken as a whole.

6.2 The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008

- Core Policy 1 - Spatial Vision and Strategic Objectives for Slough
- Core Policy 4 – Type of Housing
- Core Policy 5- Employment
- Core Policy 7 – Transport
- Core Policy 8- Sustainability and the Environment
- Core Policy 9 – Natural and Built Environment
- Core Policy 10 - Infrastructure
- Core Policy 12 – Community Safety

6.3 The Adopted Local Plan for Slough 2004 (Saved Policies)

- EMP2 Criteria for Business Developments
- H14 Amenity Space
- EN1 Standard of Design
- EN3 Landscaping
- EN5 Design and Crime
- EN17 Locally Listed Buildings
- T2 Parking
- T8 Cycling Network and Facilities

6.4 Other Relevant Documents/Guidance

- Slough Borough Council Developer's Guide Parts 1-4 2010

6.5 Emerging Local Plan Policy

The Draft Centre of Slough Regeneration Framework (Aug 2020) was presented to Members at the Planning Committee meeting of 9 September 2020 and was subsequently determined to be adopted as an evidence document for the forthcoming Slough Local Plan. The Framework promotes the redevelopment of the Queensmere and Observatory shopping centres in a way which “rediscovers the High Street” and makes it the focal point. This means that new retail and leisure uses should front onto the High Street rather than looking inwards. It also proposes to create a new pedestrian street which links the High Street to the station via Mackenzie Square and Brunel Way. This will help to break down the barrier that is currently formed by amount of traffic on the A4 Wellington Street and start to knit the centre back together. The Framework sets out the broad principles for how the centre should look in terms of building heights, street patterns, key linkages and design quality.

The Proposed Spatial Strategy was published in November 2020 and builds on the work done for the Regeneration Framework and sets out a vision and objectives along with proposals for what the pattern, scale and quality of development will be in Slough.

The Spatial Strategy involves making sufficient provision to meet housing, employment and other needs whilst at the same time conserving the natural, built and historic environment. Although it identifies broad residential areas, it does not contain any specific planning policies at this stage. Therefore the documents holds

little weight as it currently stands, and the material weight of policies would strengthen the closer the Local Plan is to adoption. It can be used to inform planning decisions but does not have the weight of planning policy.

6.6 The Planning Assessment will cover the following elements:

- Principle of the Proposed Changes
- Housing
- Design and Impact on Streetscene and Townscape
- Impact on Residential Amenity
- Highways, Access and Refuse
- Environmental Impacts

7 Planning Assessment

Principle of the Proposed Changes

7.1 The application relates to the addition of 11 homes within Blocks A and B of the original application. The table below shows the overall changes from the original application P/02683/013 and the proposed amendments of this S73 application P/02683/015:

Original Application	S73 Application
45 x 1 bed, 33 x 2 bed = Total of 78 units	51 x 1 bed, 38 x 2 bed = Total of 89 units
Flexible retail space (Class A1, A2, A3 uses) at ground floor level (593 sqm NIA) and 548 sqm (NIA).	One large commercial unit flexible Class E use (1,161 sqm NIA)
Flexible commercial floorspace at first floor fronting the High Street for either B1 (offices) or Class D2 (gym) uses	3 private accommodation units with balconies fronting the High Street
3 buildings at podium level across the site with heights of 5, 11 and 4 storeys	3 buildings at podium level across the site with heights of 5, 13 and 4 storeys
Cycle, waste and recycling storage facilities at ground floor level to the west of the site for 78 units	Increased cycle, waste and recycling storage facilities at ground floor level to the rear of the site for 89 units
Shared amenity space at first floor podium level	Shared amenity space at first floor podium level and addition of 2 roof terraces on the 11 th floor
	Amendments to layout increase staircase from Block A to High Street moved and lift to be removed

- 7.2 The procedure in Section 73 of the Town and Country Planning Act 1990 permits a developer to apply for planning permission to carry out development already authorised by an extant planning permission without complying with one or more of the conditions of that permission. If planning permission is granted, the developer obtains a new planning permission for the same development as before, but subject to different conditions.
- 7.3 The s73's powers are solely to be used to alter/remove the conditions attached to an application in order to generate a new planning permission. A s96 application was made under P/02683/014 (approved 9th August 2021) to vary the former description of development and move specific information to the existing conditions to enable a subsequent s73 application to vary these conditions. This is in line with the process adopted by Local Planning Authorities since the *Finney v Welsh Ministers* [2019] EWCA Civ 1868 ("Finney case").
- 7.4 Officers consider that the provision of additional residential units, including the proposed additional storeys to Block B, changes in appearance to Block A and flexible Class E space comprise material changes to the approved development and therefore, it is necessary to amend the approved plans and modify the wording of the relevant planning conditions to facilitate the changes to the development. Officers consider that the scale and nature of the amendments to the approved development are non-fundamental or significant in context with the overall permitted development. The appropriateness of the changes will be assessed further in the planning assessment below.
- 7.5 In conclusion, Section 73 of the Town and Country Planning Act 1990 does not provide scope to impose conditions that are so fundamentally different that they could not have been imposed on the original planning permission. Based on the information provided, it is considered that the conditions as proposed to be varied, could have been imposed on the original planning permission, do not involve changes to or raise any inconsistency with the description of development (as amended by way of s96A under ref: P/02683/014), and that such variations do not lead to a fundamental alteration of the original planning permission. The proposed variations to the conditions therefore fall within the scope of Section 73 of the Act.

Residential Uses

- 7.6 The National Planning Policy Framework 2021 encourages the effective and efficient use of land. These proposals involve the enlargement of the premises and the formation of additional residential accommodation. As such, the proposals comply with the overall thrust of the NPPF.
- 7.7 Core Strategy Policy 1 (CP1) sets the spatial strategy for the Borough and this requires all development to take place within the built up area, predominantly on previously developed land. The policy also requires the scale and density of the development to be related to the site's current or

proposed accessibility, character and surroundings. Core Policy 4 seeks high-density, non-family type housing to be located in the Town Centre. In the urban areas outside of the town centre, new residential development is expected to be predominantly family housing.

- 7.8 It is proposed to convert the Class B1/ D2 office/ gym space at first floor level within Block A into additional residential accommodation comprising three x 2 bed private residential units. It is acknowledged that the principle of the alteration was agreed for three x 2 bed affordable residential units. The benefits of additional housing would weigh in favour (in NPPF terms) and could contribute towards supporting the viability and vitality of the town centre by increasing footfall and sustaining the High Street shops and facilities. .
- 7.9 Having regard to the NPPF 2021 and the Local Development Plan, the principle of additional residential development in this location would be accepted on the condition that the scheme is of high quality and would provide an acceptable housing mix and affordable housing, which is assessed in the next section of this report.

Non Residential Uses

- 7.10 In addition to the NPPF defining commercial uses and a 'Main Town Centre Use, Policy EMP1 of Slough Borough Council's saved Local Plan policies strongly encourages employment generating use to locate within Slough Town Centre. EMP2 of the saved Local Plan policies and Core Policies 1 and 5 of the Core Strategy require proposals for new business developments to be of a high quality preserve or enhance the variety of local uses and be of a scale that is appropriate to its location.
- 7.11 The approved planning permission secured a flexible range of uses which could be accommodated within the ground floor commercial space. These uses were formalised through condition 5 attached to the planning permission. Since the scheme was approved, the Use Classes Order (2020) has been updated. Most of the approved uses (Class A1-A3 and B1) which are relevant to the scheme have now been placed within the new Class E use in order to increase the flexibility of commercial units and thereby enhance the viability and vitality of Town Centres.
- 7.12 Changing the approved uses to Class E is acceptable provided that only specific elements of Class E will occupy the unit. This amendment will not result in a physical change, aside from allowing for further flexibility including c) professional services, d) indoor sport, recreation or fitness, e) Medical or health services and f) Creche, day nursery or day centres. The recommendation would remove the ability for the ground floor units to change to Class E part g) use which includes office, research and development or industrial to ensure the uses are not harmful to the amenities of the residential uses in the area and would not give rise to unacceptable environmental impacts. The changes to the condition would prevent oermitted development Schedule 2, Part 3 Class MA (General Permitted Development) (England) Order 2015 (Updated 2021) to change Class E to Class C3 (residential) to take place.

- 7.13 The change will update the application to refer to the latest Use Class Order. A more flexible approach in terms of uses and floor space and layout would bring benefits of bringing back the site into town centre uses within an appropriate floorplate.
- 7.14 Further to the above changes, Condition 5 of the approved application will be updated in order to accommodate the alteration to the use class of the commercial unit. In addition, Condition 6 of the approved application will also be updated in order to refer to the updated class within the hours of use.
- 7.15 Overall, each of the proposed uses are deemed to be acceptable in principle and constitute non-fundamental amendments to the approved development subject to variation. The design form, housing mix, tenure and arrangement of the proposed uses are each addressed in the following sections.

Housing

Housing Mix

- 7.16 Core Strategy Policy 4 requires high-density housing to be located in Slough town centre. The policy also states that in the urban areas outside the town centre, new residential development will predominantly consist of family housing and be at a density related to the character of the surrounding area, the accessibility of the location, and the availability of existing and proposed local services, facilities and infrastructure. The supporting text states that there should be a wide choice and mix of housing to meet local needs.
- 7.17 Affordable Housing (Section 106); Developer's Guide Part 5 requires 5% of homes to be wheelchair standard on all developments of 25 or more dwellings.
- 7.18 The scheme proposes a total of 89 residential flats which comprises 51no. 1 bed units (57%) and 38no. 2 bed units (43%). 6 (7%) of the flats are wheelchair accessible units. An extract from the DAS highlighting the flat types is provided below.

SUMMARY

		Flat Type	
		1B2P	2B3P
BLOCK A		0	12
	PERCENTAGE MIX	0%	100%
	SUB-TOTAL	12	
BLOCK B		45	20
	PERCENTAGE MIX	69%	31%
	SUB-TOTAL	65	
BLOCK C		6	6
	PERCENTAGE MIX	50%	50%
	SUB-TOTAL	12	
		1B	2B
ALL BLOCKS	TOTAL UNIT TYPES	51	38
	PERCENTAGE MIX	57%	43%
	TOTAL	89	
SITE AREA		0.2435	Ha
SITE DENSITY		887	HR per Ha
NUMBER WCH UNITS		6	
PERCENTAGE WCH UNITS		7%	
NO. RESI. CYCLE SPACES		89	

7.19 The proposed amendment would provide a slightly higher percentage of 1 bed units from the consented scheme which comprised 43 no. 1 bed units (55%) and 35 no. 2 bed units (45%). Acknowledging the physical constraints on the site and that the location is generally less suitable for family units, given the lack of car parking and external garden spaces, the typology of housing is acceptable.

7.20 All proposed flats meet and exceed the minimum floor areas set out within Nationally Described Space Standards (2015) and Part 4 of the Slough Developer's Guide, which is 39sqm for a 1 bedroom dwelling and 61sqm for a 2 bedroom dwelling. The additional 1 bedroom flats on the 11th and 12th floor of Block B are all proposed 50sqm with 7.1sqm of balcony space. The additional 2 bedroom flats on the first floor of Block A are proposed between 73sqm and 83sqm with between 73sqm and 26.7sqm of amenity space. All flats benefit from dual aspect arrangements.

Affordable Housing

7.21 Core Policy 4 of the Core Strategy requires all sites of 15 or more dwellings (gross) to provide between 30% and 40% of the dwellings as social rented accommodation along with other forms of affordable housing.

- 7.22 The original consented scheme included provision for 12% affordable housing (9 x 2 bed units) which is below the Council's target of between 30-40%. The applicant contended that the development could not support additional affordable housing on the site and had provided a comprehensive Financial Viability Assessment as prepared by Turner Morum to demonstrate the economics (March 2019).
- 7.23 A Financial Viability Assessment has also been provided with this S73 application prepared by Turner Morum (August 2021), as the proposal seeks to propose 11 additional units which would reduce the affordable housing provision to 10% as a proportion. The applicant proposes to retain the same quantum of affordable housing which is secured under the s106 agreement. The same planning obligations would apply to this revised planning permission, if granted consent.
- 7.24 Turner Morum (TM) conclude that whilst the amended scheme includes 11 more residential units, it is unviable when the provision of 9 shared ownership units is provided (10% provision overall). TM conclude that the amended scheme generates a deficit when both a 12% (-£1.58m) and 10% (-£1.35m) affordable housing contribution is considered. Despite this, TM advises that the applicant is willing to proceed with the previously agreed affordable housing offer of 9 shared ownership units.
- 7.25 The Council has sought technical advice from BPS (Independent Surveyors) and is of the view that with 10% affordable housing as proposed in the amended scheme, a surplus of £83,300 can be achieved from the original consented scheme. The Council conclude overall that the surplus figure is less than 0.5% GDV and noting minimal changes to the appraisal inputs, this could reduce the surplus further. BPS advise that the scheme's viability may not justifiably support additional affordable dwellings, as the surplus is nominal and could change as a result of uncertainties within build costs.
- 7.26 Officers have considered the above housing provisions as part of the planning balance, noting that the combination of the height addition and provision of housing does not affect the level of positive weight in the balance, afforded to the previous scheme. Therefore, it is considered that there is evidence which indicates that the current proposals cannot viably sustain additional on-site provision of affordable housing above 10%, and sufficient evidence has been provided to demonstrate this position. The reduction in percentage of the affordable housing provisions does not give rise to any further material planning considerations. Therefore, the amendment to the affordable housing percentage can be considered in the context of this s73 application which seeks to amend planning conditions.

Design & Impact on Streetscene and Local Townscape

- 7.27 The NPPF states that good design is a key aspect of sustainable development and should contribute positively to making places better for

people. Development should function well and add to the overall quality of the area, establish a strong sense of place, optimise the potential of the site to accommodate development and provide for an appropriate mix of uses, respond to local character and history, create safe and accessible environments which are visually attractive.

7.28 Saved Policy EN1 requires development proposals to reflect a high standard of design and must be compatible with, and/or improve the surroundings in terms of layout, scale, height, architectural style and materials. Policy CP1 of the Core Strategy states that the scale and density of development will be related to the site's current or proposed accessibility, character and surroundings. Significant intensification of use will not be allowed in locations that lack the necessary supporting infrastructure, facilities or services or where access by sustainable means of travel by public transport, cycling and walking are limited. Policy CP8 of the Core Strategy states that all development in the Borough shall be sustainable, of a high quality design, improve the quality of the environment and address the impact of climate change.

7.29 Policy CP8 defines High Quality Design as to:

- a) Be of a high quality design that is practical, attractive, safe, accessible and adaptable;
- b) Respect its location and surroundings;
- c) Provide appropriate public space, amenity space and landscaping as an integral part of the design; and
- d) Be in accordance with the Spatial Strategy in terms of its height, scale, massing and architectural style.

Policy CP8 requires that the design of all development within the existing residential areas should respect the amenities of adjoining occupiers and reflect the street scene and the local distinctiveness of the area.

Layout

7.30 The ground floor layout of the commercial unit is proposed to be slightly increased from 1,141sqm NIA to 1,161sqm NIA. The commercial refuse store and refuse store 1 have been relocated from the western side of the site to the rear of the site, which is considered to be a simpler solution meaning that all stores can now be accessed from the loading bay on collection day without needing relocation.

7.31 The layout of the residential accommodation largely remains as per the consented scheme, with additional units in Block B and Block A. The flat units within Block A will match the layouts in the storeys above. The stairs leading from podium level down to the High Street have been reconfigured following these changes to Block A. There were previously two lifts proposed in Block A, and the amendments propose the lift to the west of the site to be removed. The gate on to the High Street will provide access for residents from the west side of the site, rather than residents entering from Herschel Street.

- 7.32 Within Block B, all additional proposed units are dual aspect. These additional levels are not an exact continuation of the layout of the storeys below. There are predominantly 6 flats of each floor and on the additional 2 floors, there will be 4 flats and two roof terraces. The roof terraces will be for the sole use of residents of Block B.
- 7.33 In light of the above, it is considered that the layout of the proposed development represents a more efficient use of the land on the ground floor and the additional units benefit from the opportunities on the primary frontages north and south, noting its linear shape.

Scale, Massing, Height

- 7.34 The proposed amendments would not change the scale, massing or height of Blocks A or C and predominantly proposes changes to the height of Block B. On the original application, height was taken of the central building Block B and Herschel Street blocks, from the original submitted scheme which comprised a 14 storey central building to the consented proposed 11 storey central building. This S73 application proposes an amendment to increase the central building Block B to 13 storeys, adding an additional 8 x 1 bedroom units.
- 7.35 It is considered that the additional proposed two storeys to Block B would be classed as full height storeys, as each floor has an internal height of approximately 2.5m, matching the floors below. The additional two storeys are set back from the floors below at Block B and would appear to be smaller in massing as a crowned addition.
- 7.36 The mass of Block B is focused centrally in the site. The revised building would still have a strong presence in selected views along the High Street and from the residential areas to the south. Given the two storey residential areas to the south, it is considered that the height of the building would result in some visual obtrusion when viewed from residential properties in Herschel Street, Victoria Street, Hencroft Street and Alpha Street North by virtue of introducing a taller element in an otherwise clear skyline. It is considered that this element of the development, in the current urban context would therefore read as a new tall building in an otherwise low-rise immediate context, however, the principle of this was accepted for the original application and the change in height of the central height would be visually minor when viewed from the surrounding area.
- 7.37 In addition, considering the cumulative impact of new developments such as 226 - 228 High Street at the Herschel Street to Alpha Street junction, officers consider that the increase in height would not result in further harm to visual amenities, which is further discussed in the Townscape section of this report.
- 7.38 Officers consider that the design of the additional two storeys which is reduced in massing from the floors below would better articulate the

upper floors of the development, and would create added visual interest to the skyline.

- 7.39 It is considered that the additional 2 storeys would only result in localised impacts on the surrounding area, which officers consider would be very modest, contained and outweighed by the public benefits of the scheme, in terms of contributing to the regeneration of the site and wider Town Centre area with regards to relevant planning policy and material considerations. The increased scale of development proposed reflects the emerging character of the Town Centre and can be suitably accommodated within the depth of the site. The proposed amended height and massing are therefore considered to be in accordance with Policy EN1 of the Saved Local Plan Policies and Core Policy 8 of the Core Strategy.

Material and Façade Treatment

- 7.40 The main façade changes are proposed to Block A, where 3 additional units are proposed with balconies to the High Street. 198 High Street and reinforces the line of the High Street. The brickwork will maintain the surface interest and will be similar to the adjacent historic buildings.
- 7.41 The double-height frame in reddish brick which surrounds the ground and first floor windows has been adapted to suit new fenestration. Balconies are introduced on first floor as a result of the proposed amendment from the commercial provision on the first floor to residential on the first floor.
- 7.42 The level at which the light grey brick transitions into the reddish brick has been maintained. Officers consider that this ensures continuity with the adjacent building, Planning Officers consider that the general principle of the High Street elevation is intact considering the changes to Block A.



7.43 With regards to Block B, the principles of the consented scheme will be maintained, as the taller blocks will be finished in a light coloured brick and the shorter elements will have a reddish brick finish. The additional two floors at the top of the central building articulate the roof line and add further visual interest to the development.

7.44 With regards to Block C, the elevation facing Herschel Street will not materially change from the consented scheme.

Townscape

7.45 Lichfields have prepared a Townscape and Visual Impact Assessment (TVIA) Addendum which sets out a detailed analysis of the site context, identifying a series of current emerging townscape character areas upon which the impacts of the application scheme are assessed.

7.46 The following summarises the proposed amended scheme's effects on each of the Townscape Character Areas:

- Slough Town Centre TCA: Minor Beneficial
- Residential TCA: Negligible to Minor Adverse
- Heart of Slough TA: Negligible
- Wellington Street Road and Rail Corridor TCA: Negligible

This confirms that the proposed changes would only have a negligible to minor adverse impact to the residential TCA, predominantly from the area south of Herschel Street given the difference in scale and character. However, it is considered there is little change from the original application, which also concluded there to be a negligible to minor adverse impact to the residential TCA and given the cumulative effect of

new developments, this would not be significantly detrimental to the townscape of the area.

- 7.47 The TVIA includes a series of verified views of the Site, including Accurate Visual Representations of views in either rendered or wireframe form. It was agreed with the applicants at the pre-application stage that 4 of the original 15 representative viewpoints would need to be considered from the surrounding area, as the original TVIA dated December 2018 assessed a scheme that was 15 storeys in height, which was then reduced in height during the determination period.
- 7.48 The assessment includes the following viewpoints and the cumulative townscape effects taking into consideration :
- View 1 - High Street (entrance to Observatory Shopping Centre) – moderate beneficial as no. 226-228 High Street will provide screening
 - View 2 - Herschel Street adjacent to Alpha Street junction – moderate beneficial as no. 226-228 High Street would screen the majority of the built form of Block B
 - View 5 - Herschel Street adjacent to Victoria Street junction- minor neutral as there are no visible cumulative schemes that would affect the visibility of the proposed scheme from this view location.
 - View 7 - Albert Street (south of St.Mary's Church)- minor adverse as there are no visible cumulative schemes that would affect the visibility of the proposed scheme from this view location.
- 7.49 The proposed development would not have an overall detrimental impact upon the character and appearance of the surrounding area or buildings. Officers consider that any localised impacts on the surrounding area would be modest, contained and outweighed by the public benefits of the scheme, in terms of contributing to the regeneration of the site and wider Town Centre area with regards to relevant planning policy and material considerations.
- 7.50 It is considered that the proposed development would result in a high degree of change as it would have a greater visibility than the existing building. Officers consider that the development as amended, including the increase in height on Block B by two storeys would cause some further minor adverse effects to the townscape character areas and to visual receptors to result from the development. It is acknowledged that from some views as assessed in the TVIA Addendum, that there would be some beneficial cumulative impacts when taking into consideration future development. The High Street elevation of Block A is broadly consistent with the approved scheme as the ground floor commercial unit and first floor residential accommodation are defined within the streetscene with a double height frontage which extends to match the roofline of the adjacent Locally Listed Building and is sensitive to the building's ridge height and line.
- 7.51 In conclusion, the design and resulting appearance of the development is considered to be acceptable, on balance, noting that the proposals result in some harm to the local townscape character by virtue of the scale and

height of the central building and its relationship with the immediately neighbouring low rise buildings, however the additional 2 storeys to Block B does not result in any significant harm than the consented scheme. The scheme, as amended, would therefore be considered to constitute an acceptable design and an appropriate form of development on this site.

- 7.52 Notwithstanding this, it is recognised that the scale, massing and height has been optimised to enable a sustainable mix of uses and delivery of housing with adequate servicing, delivery and parking provisions. In conclusion, it is the Councils view that the level of harm to the townscape is not significant enough to warrant a refusal on townscape grounds, on balance. It is considered that the development is capable of achieving a sufficient level of compatibility with the surrounding townscape which, over time, would address the criteria in the Local Plan Saved Policy EN1, Core Strategy Policy 8 and Core Strategy Policy 9 and the NPPF which seeks to deliver well designed places.

Future Development

- 7.53 In conclusion, it is considered that the proposed amendments would safeguard the potential to develop adjoining sites and may not unreasonably prejudice or restrict schemes coming forward in the future, either on a standalone piecemeal basis or a comprehensive basis whereby specific sites are assembled and brought forward together.

Impacts on Residential Amenity

- 7.54 EN1 of the Local Plan and Core Policy 8 of the Core Strategy requires new development proposals to reflect a high standard of design and to be compatible with and/ or improve the surroundings in terms of the relationship to nearby properties. The NPPF at paragraph 130 sets out guiding principles for the operation of the planning system. One of the principles set out is that authorities should always seek to secure high quality design and a good standard of amenity for all existing and future occupants of land and buildings. In considering the proposals, officers have had due regard for the impacts on the amenity and living conditions of residents residing within adjoining and adjacent dwellings in the High Street, Herschel Street, Alpha Street, Hencroft Street, Victoria Street and Park Street with regards to the impact on cumulative levels of daylight, sunlight, overshadowing and privacy as a result of the proposed amendments.
- 7.55 The daylight, sunlight and overshadowing addendum has been submitted by the applicant to assess the effects of the proposals on the daylight and sunlight levels to adjoining occupiers. The technical analysis has confirmed that the daylight availability to the great majority of neighbouring buildings would be retained in accordance with the BRE recommendations. The Report confirms that 98.61% of habitable rooms would receive daylight in accordance with BRE recommended values which is considered to demonstrate a very high level of compliance, noting the urban town centre context. The proposed increase in height to

Block B would not significantly worsen the daylight conditions within neighbouring buildings any further than the consented scheme.

- 7.56 Sunlight availability to neighbouring residential properties that face within 90° of south demonstrates that BRE's recommended values would be satisfied in the majority of the locations. Within the consented scheme at 190-192 High Street, 18 windows would fall below the BRE recommendation for annual sunlight and 19 windows would fall short of the winter sunlight recommendation out of a total of 74 windows tested. To the first and second floor windows in 198-200 High Street which were already assessed in the consented application as being likely to comprise bathrooms or kitchens, and are less reliant on access to sunlight. The properties are also served by a second aspect with windows which would be BRE compliant in respect of daylight levels. There is one window at 16 Park Road which would have the winter sunlight reduced from 1% to 0%, however the difference would be unnoticeable to the occupants. Overall, the quantity of sunlight to proposed amenity areas is good and would satisfy BRE recommended values.
- 7.57 In relation to overshadowing, the results confirm that the BRE recommendation of two hours of sunlight on 50% of the area on 21 March would be satisfied for the High Street. There are no material differences in the results from the report in the consented scheme and the submission of the amended plans.
- 7.58 With regards to the distances between the new development blocks and existing facing buildings is considered to be typical of central urban infill developments and this would not change from the consented scheme. There are no residential windows within the properties on High Street and so the additional 3 units and associated balconies will not result in any overlooking.

Crime

- 7.59 Paragraph 91 of the NPPF requires planning decisions to promote safe and accessible environments where crime and disorder, and the fear of crime, do not undermine quality of life or community cohesion. These objectives are consistent with Core Strategy Policies 8 and 12, and Local Plan Policy EN5 which requires development proposals to be designed to reduce the potential for criminal activity and anti-social behaviour.
- 7.60 This has been addressed within the scheme which provides secure entrances, separation of cycle storage with the commercial servicing areas, and a separation of the commercial first floor with the residential components which ensure there will be no conflicts between the different uses. The provisions incorporated into the scheme were assessed by the Crime Prevention Design Advisor as being acceptable for the original scheme and the proposed amendments will maintain the level of safety and security of the dwellings. The active frontages of the first floor accommodation on the High Street would increase natural surveillance.

Highways, Access and Refuse

- 7.61 Paragraph 104 of the NPPF 2021 states that in assessing specific applications for development, it should be ensured that opportunities to promote walking, cycling and public transport use are identified and pursued and the environmental impacts of traffic and transport infrastructure can be identified, assessed and taking into account0 including appropriate opportunities for avoiding and mitigating any adverse effects.

a) Appropriate opportunities to promote sustainable transport modes can be – or have been – taken up, given the type of development and its location;
b) Safe and suitable access to the site can be achieved for all users; and
c) Any significant impacts from the development on the transport network (in terms of capacity and congestion), or on highway safety, can be cost effectively mitigated to an acceptable degree
- 7.62 Paragraph 111 of the NPPF 2021 states that development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe. This is reflected in Core Policy 7 and Local Plan PoliciesT2 and T8.
- 7.63 Paragraph 112 of the NPPF 2021 states that development should give priority first to pedestrian and cycle movements and second to public transport. Applications should create places that are safe, secure and attractive, allow for the delivery of goods, access by service and emergency vehicles and charging of plug-in vehicles.
- 7.64 A Transport Statement Addendum has been prepared by RSK outlining a travel plan, servicing/ waste management plan and fully assesses the impact of the scheme on trip generation in the Town Centre. Given site’s sustainable location within the town centre and combined with the easy access to alternative public transport modes in vicinity of the site, it is considered that a car free development would be acceptable.
- 7.65 The Highways Officer was consulted and a response was received on 1st October 2021 confirming there are no significant changes proposed to the application that was approved at the October 2019 planning committee. The most significant changes to the proposal from a Highways and transport perspective are in the following areas;

Pedestrian Access
- 7.66 It is noted that the pedestrian access to elements of the proposed building have been provided from the High Street elevation, this was previously for the commercial access and for fire escape purposes only. The access off the High Street now serves some of the residential units as well as serving as a fire escape. It is considered that this is an

improvement and it provides a more active frontage to the high street with some units gaining access off the High Street rather than from Hershel Street. Although this means all cyclist access is now from Hershel Street, this does not have any highways or transport implications.

Commercial Access

- 7.67 The Commercial Unit has access to the rear service yard where it would be getting it's deliveries from and having refuse collected from. This is considered to be acceptable.

Cycle Parking

- 7.68 The configuration and layout of cycle parking for this development has been altered with this proposal and the number of cycle parking spaces being provided is adequate. The changes in layout have meant that there is a reduction of Sheffield stands and increase in 2 tier cycle parking for the cycle parking being provided to both blocks B and C. In the absence of any individual cycle lockers, Highways Officers recommend that a reasonable mix of Sheffield stands should be provided to compliment the 2-tier cycle parking, which are not always easy for all users to utilize. Therefore, there must be a reasonable provision of Sheffield stands within the secure cycle stores for less able bodied persons or those with limited mobility to use as these provisions encourage the maximum cyclist uptake. In order to satisfy the local highway authority, the minimum number of Sheffield stand cycle parking spaces for block B must be increased back to 9 as per the previously consented scheme and a provision of Sheffield stands must be introduced for the Block A cycle parking, it is suggested that 4no cycle parking spaces using Sheffield stands. Furthermore the cycle store for block B appears to be very congested for the 60no cycle parking spaces shown, and may need to be sub divided for enhanced security and to encourage maximum usage.
- 7.69 12no cycle parking spaces have been shown for the commercial units with 4no per unit however these have been shown in a single location and without sufficient details on the type of cycle parking. As this cycle store could serve more than one individual commercial unit, the cycle parking must be secured and segregated between the individual users. Highways Officers query whether such an arrangement can be provided in this location and have asked the applicant to provide further information on this aspect to ensure the cycle parking for the various commercial units can be securely segregated.
- 7.70 Although Officers are satisfied with the numbers of cycle parking, the applicant is advised to provide further details on the cycle parking as well as on the cycle stores themselves to ensure Highways Officers are satisfied that they can operate safely and effectively, giving residents secure and convenient cycle parking facilities. These details to be secured by way of pre-commencement conditions or by submitting a modified plan detailing the above amendments.

Refuse

- 7.71 The revised proposal does not significantly change the refuse collection arrangements from the approved scheme, save for the relocation of the refuse store for Block A. The relocation of the block A refuse store means that the management company no longer needs to move bins on collection day and that they can be collected directly from the bin stores which improves the situation logistically.
- 7.72 The commercial refuse bins are now in a location where they better suit the amended layout for the occupiers of these units whilst also being more convenient for waste collection purposes which is considered to be acceptable. The capacities of the refuse bin allocation has not changed nor has the floor area of the commercial units. However, Highways Officers note the bin stores may need to be increased in size to avoid bins being stored in the servicing area. It would be preferred if further details of the refuse storage and collections were provided within the refuse strategy as per the condition.
- 7.73 The Highways officer raises some concern with the carrying distances for residents from block A and it is not clear that the route for them to carry their bins from the stairwells or lift cores will be suitable. Officers do not consider the amendments to the layout substantially change the approved layout although it is noted slight revisions to reduce carrying distances would comprise an improvement.
- 7.74 Given the above, further details on the refuse management strategy will be required which can be secured by pre-commencement condition.

Servicing

- 7.75 There have been no notable changes to the servicing arrangements or their operation and as such we have no further comments on this matter.

Summary

- 7.76 The amended proposal is not vastly different from the approved scheme and therefore there are no objections to this scheme from a highway's perspective. Additional information has been requested by the Highway's officers including on commercial access, the refuse management strategy and the design and layout of the cycle storage/ parking. It is considered that the additional information requested on the refuse management strategy can be sufficiently addressed through the existing pre-occupation condition 7 (bin storage) that will remain as per the original application.

Environmental Impacts

Noise and Air Pollution

- 7.77 Core Policy 8 states that development should not give rise to unacceptable levels of pollution including noise and air pollution and should not be located in noisy environments unless the development includes appropriate mitigation measures to limit the adverse effects on occupiers.
- 7.78 A letter dated 8th July 2021 from the noise consultant states that the changes are unlikely to make any material changes to the noise assessment and that the original noise survey is likely to be representative of the current noise climate of the site and therefore the applicants have not repeated the noise survey and any subsequent mitigation would remain valid.
- 7.79 It is considered that the proposed amendments would be in accordance with the information assessed from the original consented scheme. Subject to sufficient information being provided to satisfy the same environmental conditions secured under the previous planning permission, and revised conditions as proposed in this current application, that the policy requirements of Core Policy 8 and the guidance in the NPPF would be met.

Flood Risk and Water

- 7.80 Core Policy 8 relates to flood risk and sets out that new development will only be permitted where it is safe and it can be demonstrated that there is minimal risk of flooding to the development. A Sustainable Urban Drainage Strategy has been prepared which considers various drainage techniques for the site, which shows little difference from the consented application.
- 7.81 The LLFA have stated that the proposed changes will not affect the conditions previously set for foul/ surface water drainage to comply with Core Policy 8 and the NPPF i.e. they can remain outstanding.

Sustainability and Energy

- 7.82 Requirements relating to sustainability are set out in Core Policy 8. This expects all development within the Borough to be of a high quality design, improve the quality of the environment and address the impact of climate change. The policy sets out a number of sustainable design principles that are expected to be addressed within proposals for new development.
- 7.83 The Energy Statement and Sustainability Statement do not show a change from the consented development which was designed to target a 15% reduction from 'regulated emissions' as per the Building Regulations Part L and that the ground floor and first floor may achieve 'Very Good'

BREEAM rating including all mandatory requirements in accordance with Core Policy 8 and the NPPF 2021.

Fire Safety

- 7.84 The fire strategy for the original proposed development would not change substantially considering the proposed amendments to the layout. This has been carefully considered given the constrained nature of the site. Buildings A and B are accessed via the first floor podium and given the extended travel distances from the fire service access points, these blocks will be fitted with sprinkler systems. Further to this, a fire service refuge area has been allocated to the entrance level of Block B. The circulation core to building C continues to ground floor level, ensuring that travel distances from the fire appliance to the base of stair are compliant. The commercial and retail areas will each be provided with sprinkler systems and fire escapes to the high Street frontage and rear exits to the service yard and Herschel Street. The Council's Building Control Officer has been consulted in respect of Fire Safety issues and has provided no comment on the application. Notwithstanding this, it is considered that a planning condition could be secured which requires the development to be carried out in accordance with the measures set out in the Fire Strategy.
- 7.85 It is concluded that a satisfactory standard of residential accommodation could be provided within the development subject to the quality of construction, materials and fittings within the building.

Ecology

- 7.86 Paragraph 170 of the NPPF requires new development to minimise impacts on biodiversity and provide net gains in biodiversity. Core Policy 9 relates to the natural environment and requires new development to preserve and enhance natural habitats and the biodiversity of the Borough, including corridors between biodiversity rich features. Saved Policy EN22 sets out that special account will be taken of nature conservation interest when determining proposals for development which would be detrimental to land which contains features of ecological importance. Ecological appraisals are required where proposed development is likely to threaten any nature conservation interest.
- 7.87 An updated Ecological Constraints and Preliminary Roost Assessment Report has been provided with the S73 application, dated July 2021.
- 7.88 Subject to conditions, that were imposed on the original application and would still apply, the proposals the scheme is capable of achieving compliance with Core Policy 9 and saved Policy EN22 and other relevant national planning policies an industry guidance with respect to ecology. Overall, there are no overriding ecological constraints to the development of the site.

Section 106

- 7.89 Core Policy 10 of the Core Strategy states that development will only be allowed where there is sufficient existing, planned or committed infrastructure. All new infrastructure must be sustainable. Where existing infrastructure is insufficient to serve the needs of new development, the developer will be required to supply all reasonable and necessary on-site and off-site infrastructure improvements.
- 7.90 The developer has entered into a Section 106 Agreement for the consented scheme, including planning obligations for affordable housing, an off-site EV Car Club, recreation, education, safeguard Herschel Street, travel plans and employment and training initiatives.
- 7.91 As a result of the additional 11 units as proposed in the s73 application, the s106 contributions are required to be updated from the original consented scheme, including open space (£300 per dwelling) and education contributions (51 x 1 bed £903 per dwelling and 38 x 2 bed (£4,828 per dwelling)).

	Original Scheme	Amended Scheme
Car Club	£50,000	£50,000
Open Space/ Recreation	£23,330	£26,700
Education	£180,055	£229,517
Total S106	£253,385	£306,217

- 7.92 Based on the information assessed to date, such changes to the obligations would be considered to comply with Regulation 122 of The Community Infrastructure Levy Regulations 2010 in that the obligations are considered to be:
 - (a) necessary to make the development acceptable in planning terms;
 - (b) directly related to the development; and
 - (c) fairly and reasonably related in scale and kind to the development.

PART C: RECOMMENDATION

PART D: CONDITIONS

- 1. Time Limit
The development hereby permitted shall be commenced within three years from the date of this permission.

REASON: To prevent the accumulation of planning permissions, and to enable the Council to review the suitability of the development in the light of altered circumstances and to comply with the provisions of Section 91 of the Town and Country Planning Act 1990.

2. The development hereby approved shall be implemented only in accordance with the following plans and drawings hereby approved by the Local Planning Authority

Ground Floor Plan HSL P100 S2-P12
First Floor Plan HSL P101 S2-P8
Second Floor Plan HSL P102 S2-P6
Third Floor Plan HSL P103 S2-P6
Fourth Floor Plan HSL P104 S2-P7
Fifth Floor Plan HSL P105 S2-P7
Sixth Floor Plan HSL P106 S2-P6
Seventh Floor Plan HSL P107 S2-P6
Eighth Floor Plan HSL P108 S2-P8
Ninth Floor Plan HSL P109 S2-P8
Tenth Floor Plan HSL P110 S2-P8
Roof Plan AHSL P114 S2-P9
High Street Elevation HSL P200 S2-P7
Herschel Street Elevation HSL P201 S2-P7
Section AA, BB & CC HSL P300 S2-P6
Section DD & EE HSL P301 S2-P6
Section FF HSL P302 S2-P6
Section GG HSL P303 S2-P5
Section HH HSL P304 S2-P8

REASON: For the avoidance of doubt, to ensure that the site is developed in accordance with the submitted application and to ensure that the proposed development does not prejudice the amenity of the area to comply Policy EN1 of The Local Adopted Plan for Slough 2004, Core Policy 9 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2021).

Condition varied to include latest submitted drawings

3. Details and Samples of materials

The proposed development shall be carried out in accordance with the following materials:

- Rega by Vandersanden or similar with light grey mortar joint
- Ivahoe Cream by Ibstock or similar with light brown mortar joint-
- White glazed brick with light grey mortar joint
- The proposed powder coated window framing system sample and balcony panels/balustrades is to comprise RAL 7006 (Champagne)

REASON: To ensure a satisfactory appearance of the development so as not to prejudice the visual amenity of the locality in accordance with Policy EN1 of The

Adopted Local Plan for Slough 2004, Core Policy 9 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2019).

4. Architectural details

No development shall commence above ground floor level until full architectural detailed drawings at a scale of not less than 1:20 (elevations, plans and sections) of windows (including surroundings and reveals), down pipes, gutters, edging details to flat roofs, lift over-runs, balustrades and balconies have been submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the approved details and retained thereafter.

REASON: To ensure a satisfactory appearance of the development so as not to prejudice the visual amenity of the locality in accordance with Policy EN1 of The Adopted Local Plan for Slough 2004, Core Policy 9 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2019).

5. Approved Uses

Notwithstanding the information in the approved plans and subject to the provisions within the relevant conditions set out in this planning permission, the following specified land uses are permitted within the specified floorspace:

Notwithstanding the information in the approved plans and subject to the provisions within the relevant conditions set out in this planning permission, the following specified land uses under Class E (Commercial) are permitted within the specified floorspace:

- a) retail, other than hot food;
- b) food and drink;
- c) professional services;
- d) indoor sport, recreation or fitness;
- e) Medical or health services; and
- f) Creche, day nursery or day centre.

As set out in Class E, Part 3, Schedule 2 of the General Permitted Development Order 2015 (as amended) or the provisions of the relevant Class/Part upon implementation, this permission benefits from a period during which changes of use of the above specified floorspace between uses Class E as set out in the description of development may take place without the need for further planning permissions. Permitted development under Class MA, Part 3, Schedule 2, (General Permitted Development) (England) Order 2015 to change Class E to Class C3 (residential) are removed. This flexibility is for a period of ten years from the date of the approval.

Reason: To ensure the uses are compatible with the adjoining land uses and to ensure that the amenity of occupiers residing in surrounding residential

properties would be safeguarded in accordance with policies EMP2 and OSC15 of The Adopted Local Plan for Slough 2004, Core Policies 1, 5 and 6 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2021).

Condition varied to refer to latest Class Use Order.

6. Hours of Operation

(A) The Class E uses hereby permitted shall operate only between 0700 hours and 2300 hours, on weekdays and on Saturdays and on 0700 hours to 2230 hours on Sundays and Bank Holidays.

Reason: To ensure that the amenities of surrounding occupiers are not unduly affected by noise and other disturbance, in accordance with Policy EMP2 and OSC15 of The Adopted Local Plan for Slough 2004, Core Policies 1 and 6 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2021).

Condition varied to refer to latest Class Use Order.

7. Bin storage

Prior to first occupation of the development, a management strategy ('the strategy') to be used by the management company for the transfer of waste/recycling bins to collection points and the collection of bins shall be submitted to and approved in writing by the Local Planning Authority. The waste/recycling storage facilities shall be provided in accordance with the approved drawings and shall be retained at all times in the future for this purpose, and the strategy shall be complied with for the duration of the development.

REASON: In the interests of visual amenity of the site and in the interests of highway safety and convenience in accordance with Policy EN1 of The Adopted Local Plan for Slough 2004, Core Policy 8 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2019).

8. Cycle parking

The cycle parking racks and storage facilities within the development shall be provided in accordance with the approved plans. The cycle facilities shall be implemented prior to the occupation of the relevant part of the development and shall be retained thereafter at all times in the future for this purpose.

REASON To ensure that there is adequate cycle parking available at the site in accordance with Policy T8 of The Adopted Local Plan for Slough 2004, to meet the objectives of the Slough Integrated Transport Strategy, Core Policy 7 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2019).

9. Lighting Scheme

Construction of the development above the ground level shall not commence until details of a lighting scheme (to include the location, design of lighting features, the nature and levels of illumination) has been submitted to and approved in writing by the Local Planning Authority and the scheme shall be implemented prior to first occupation of the development and maintained in accordance with the details approved.

REASON: To ensure that a satisfactory lighting scheme is implemented as part of the development in the interests of residential and visual amenity and in the interest of crime prevention to comply with the provisions of Policies EN1 and EN5 of The Adopted Local Plan for Slough 2004 and policy 12 of the adopted Core Strategy 2006-2026 and the National Planning Policy Framework (2019).

10. Boundary treatment, walls, fences and gates

Construction of the buildings above ground floor level shall not commence on site until details of the proposed boundary treatment including position, external appearance, height and materials of all boundary walls, fences and gates have been submitted to and approved by the Local Planning Authority. The development shall not be occupied until the approved boundary treatment has been implemented on site. It shall be retained at all time in the future.

REASON: In the interests of the visual amenity of the area and to reduce opportunities for crime and anti-social behaviour in accordance with Policies EN1 and EN3 of The Adopted Local Plan for Slough 2004, Core Policies 1 and 8 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2019).

11. Landscaping Scheme

Prior to commencement of works relating to the ground floor courtyard or external spaces, a detailed landscaping scheme has been submitted to and approved in writing by the Local Planning Authority. This scheme should include the type, density, position and planting heights, along with staking/guying, mulching, feeding, watering and soil quality, of any new trees and shrubs, and details of hard-surfaces which shall include compliance with the surface water drainage mitigation as approved under condition 12 of this planning permission.

On substantial completion of the development, the approved scheme of hard

landscaping shall have been constructed. The approved scheme of soft landscaping shall be carried out no later than the first planting season following completion of the development. Within a five year period following the implementation of the scheme, if any of the new or retained trees or shrubs should die, are removed or become seriously damaged or diseased, then they shall be replaced in the next planting season with another of the same species and size as agreed in the landscaping tree planting scheme by the Local Planning Authority.

REASON In the interests of the visual amenity of the area and accordance with Policy EN3 of The Adopted Local Plan for Slough 2004 and to ensure that surface water discharge from the site is satisfactory and shall not prejudice the existing sewerage systems in accordance with Policies 8 and 9 of the adopted Core Strategy 2006 - 2026 and the National Planning Policy Framework (2019).

12. Management & Maintenance of Surface Water Drainage Scheme

Prior to commencement of works, details of the Surface Water Drainage Plan and a Whole Life Management and Maintenance Scheme for these measures shall be submitted to and approved in writing by the Local Planning Authority. The Surface Water Drainage Scheme shall include the following:

- BRE 365 soakage tests to be undertaken to determine the soakage rate of the soil.
- Method of the treatment of the surface water.
- Information evidencing that the correct level of water treatment exists in the system is required in accordance with the Ciria SuDS Manual C753
- Whole Life Management and Maintenance Plan of the SUDs

The Surface Water Drainage measures and Whole Life Management and Maintenance Scheme shall be implemented in accordance with the approved details prior to first occupation of the development hereby permitted, and will thereafter be permanently retained and maintained.

Reason: To ensure that surface water discharge from the site is satisfactory and shall not prejudice the existing sewerage systems in accordance with Policies 8 and 9 of the adopted Core Strategy 2006 – 2026, the Council’s Development Plan Document – Developer’s Guide Part 4 Section 6 (2016) and the National Planning Policy Framework (2019).

13. Noise attenuation and ventilation

Prior to the commencement of any works above ground level, detailed specifications of the Mechanical Ventilation for the commercial ground and first floor units, the flats including the internal servicing/loading and parking areas and glazing systems shall be submitted to the local planning authority for approval in writing. The details will demonstrate that the glazing performance will have an Rw of 31-33 dB or more in accordance with BS 8233. The ventilation system must be designed to ensure that noise generated by the system, when combined with noise from external sources, does not exceed the following maximum noise levels, within internal rooms, in accordance with BS 8233 which are as follows:

- Bedrooms NR20
- Living Rooms NR25
- Bathrooms/kitchens NR30-35
- Corridors NR30-35

No dwelling or commercial ground/first floor unit shall be occupied until its attenuation and ventilation mitigation measures have been installed in accordance with the approved details. The approved mitigation measures shall be retained thereafter.

REASON: In the interest of safeguarding the living conditions of residents within the development and of staff/users within the commercial units and servicing, car parking and loading area, to reduce noise pollution and to ensure the satisfactory ventilation of rooms and internal areas when windows are closed, to comply with policy 8 of the Core Strategy 2006 - 2026 adopted 2008 and the National Planning Policy Framework (2019).

14. Designing out crime

No development above ground floor level shall commence until evidence has been provided to demonstrate how the applicant has used best endeavours to incorporate measures to comply with Secured by Design Gold Award. The evidence shall be submitted and approved in writing by the Local Planning Authority, prior to commencement of any development above ground. The development shall be carried out in accordance with the approved Secured by Design Application, and shall not be occupied or used until the Council acknowledged in writing that it has received written confirmation of compliance. The approved security measures shall be maintained and retained thereafter.

REASON: In order to minimise opportunities for crime and anti-social behaviour in accordance with Policy EN5 of The Adopted Local Plan for Slough 2004 and Core Policies 8 and 12 of the adopted Core Strategy 2006-2026 and the National Planning Policy Framework (2019).

15. Internal loading

Prior to first occupation of the relevant part of the development, the internal loading and turning provisions for the relevant part of the development shall be provided in accordance with approved plans and retained thereafter.

REASON: To ensure that the proposed development does not prejudice the free flow of traffic or conditions of general safety on the local highway network in accordance with Policy T3 of The Adopted Local Plan for Slough 2004, Policies 7 and 10 of the adopted Core Strategy 2006-2026 and the guidance contained in the Council's Developer's Guide Part 3 (2008) and the National Planning Policy Framework (2019).

16. Sustainable Development

The proposed energy efficiency and low carbon measures incorporated within the development shall be carried out in accordance with the Energy Statement Update by RSK (dated September 2019) which calculates the reduction in annual CO2 emissions to 19.10 tonnes corresponding to a reduction across the site of 15% over Building Regulations 2013 Part L and associated Approved Documents.

No part of the Development shall be used or first occupied (other than for construction purposes) until it has been carried out in accordance with the approved details and shall be retained for the lifetime of the development.

REASON: In the interest of sustainable development in particular reducing carbon emissions and in accordance with policy 8 of the Core Strategy 2008 and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2019).

Energy Statement Compliance (Residential)

17. Within 3 months of final occupation of the residential dwellings within a Development block, evidence shall be submitted that the energy statement approved pursuant to Condition 16 above have been implemented in accordance with the approved details.

Reason: In the interests of energy conservation and reduction of CO2 emissions, in accordance with policies CP8 and the National Planning Policy Framework (2019).

Energy Statement Compliance (Non-residential)

18. Within 3 months of final occupation of the non-residential units within the Development, evidence shall be submitted that the energy statements approved pursuant to Condition 16 above have been implemented in accordance with the approved details.

Reason: In the interests of energy conservation and reduction of CO2 emissions, in accordance with policies CP8 and the National Planning Policy Framework (2019).

19. Green Roofs

Prior to the commencement of work on the relevant part of the buildings hereby approved, details of green roofs, including planting and maintenance schedules, and ecological enhancement measures for the development shall be submitted to and approved in writing by the Local Planning Authority. The green roofs shall be laid out on the relevant building, prior to first occupation within the relevant building in accordance with the details as approved. The green roofs shall be permanently retained thereafter.

Reason: To ensure the provision of green roofs in the interests of sustainable urban drainage and habitat provision, in accordance with policies policy 8 of the Core Strategy 2008 and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2019).

20. Construction Management Scheme

No development shall take place until a Construction Management Plan has been submitted to and approved in writing by the local planning authority, which shall include details of the provision to be made to accommodate all site operatives', visitors' and construction vehicles loading (to a minimum Euro 6/VI Standard), off-loading, parking and turning within the site and wheel cleaning facilities during the construction period, non-road mobile machinery (NRMM) controls to be in line

with Table 10 in the Low Emission Strategy (LES) guidance and that all heating systems (when the development is operational) shall meet the emission standards laid out in table 7 of the LES guidance. The Plan shall thereafter be implemented as approved before development begins and be maintained throughout the duration of the construction works period.

REASON In the interest of minimising danger and inconvenience to highway users and in the interests of air quality in accordance with policies 7 and 8 of the Core Strategy 2008 and the National Planning Policy Framework (2019).

21. Construction and Demolition Plan

No development shall begin until details of a scheme (Working Method Statement) to control the environmental effects of demolition and construction work has been submitted to and approved in writing by the Local Planning Authority. The scheme shall include:

- (i) control of noise
- (ii) control of dust, smell and other effluvia
- (iii) control of surface water run off
- (iv) site security arrangements including hoardings
- (v) proposed method of piling for foundations

The development shall be carried out in accordance with the approved scheme or otherwise, as agreed by the Local Planning Authority.

REASON: In the interests of the amenities of the area in accordance with Core Policy 8 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance set out in the National Planning Policy Framework (2019).

22. Phase 1 Desk Study and Preliminary Risk Assessment

Development works shall not commence until a Phase 1 Desk Study (DS) has been submitted to and approved in writing by the Local Planning Authority. The Phase 1 Desk Study shall be carried out by a competent person in accordance with Government, Environment Agency and Department for Environment, Food and Rural Affairs (DEFRA) guidance and approved Codes of practices, including but not limited to, the Environment Agency model procedure for the Management of Land Contamination CLR11 and Contaminated Land Exposure Assessment (CLEA) framework, and CIRIA Contaminated Land Risk Assessment Guide to Good Practice C552. The Phase 1 Desk Study shall incorporate a desk study (including a site walkover) to identify all potential sources of contamination at the site, potential receptors and potential pollutant linkages (PPLs) to inform the site preliminary Conceptual Site Model (CSM) and Preliminary Risk Assessment (PRA).

REASON: To ensure that the site is adequately risk assessed for the proposed development and in accordance with Policy 8 of the Core Strategy 2008.

23. Phase 2 Intrusive Investigation Method Statement

Should the findings of the Phase 1 Desk Study approved pursuant to the Phase 1 Desk Study condition identify the potential for contamination, development works shall not commence until an Intrusive Investigation Method Statement (IIMS) has been submitted to and approved in writing by the Local Planning Authority. The IIMS shall be prepared in accordance with current guidance, standards and

approved Codes of Practice including, but not limited to, BS5930, BS10175, CIRIA C665 & C552 and BS8576. The IIMS shall include, as a minimum, a position statement on the available and previously completed site investigation information, a rationale for the further site investigation required, including details of locations of such investigations, details of the methodologies, sampling and monitoring proposed.

REASON: To ensure that the type, nature and extent of contamination present, and the risks to receptors are adequately characterised, and to inform any remediation strategy proposal and in accordance with Policy 8 of the Core Strategy 2008.

24. Remediation Validation

No development within or adjacent to any area(s) subject to remediation works carried out pursuant to the Phase 3 Quantitative Risk Assessment and Site Specific Remediation Strategy condition shall be occupied until a full Validation Report for the purposes of human health protection has been submitted to and approved in writing by the Local Planning Authority. The report shall include details of the implementation of the remedial strategy and any contingency plan works approved pursuant to the Site Specific Remediation Strategy condition above. In the event that gas and/or vapour protection measures are specified by the remedial strategy, the report shall include written confirmation from a Building Control Regulator that all such measures have been implemented.

REASON: To ensure that remediation work is adequately validated and recorded, in the interest of safeguarding public health in accordance with Policy 8 of the Core Strategy 2008 and the National Planning Policy Framework (2019).

25. Watching Brief

The developer shall carry out a watching brief during site work and shall draw to the attention of the Local Planning Authority to the presence of any unsuspected contamination (to soil or/and water, determined by either visual or olfactory indicators) encountered during the development. In the event of contamination to land and/or water being encountered, no development or part thereof shall continue until a programme of investigation and/or remedial work to include details of the remedial scheme and methods of monitoring, and validation of such work undertaken has been submitted to and approved in writing by the Local Planning Authority. None of the development shall be commissioned and/or occupied until the approved remedial works, monitoring and validation of the works have been carried out and a full validation report has been submitted to and approved in writing by the Local Planning Authority. In the event that no significant contamination is encountered, the developer shall provide a written statement to the Local Planning Authority confirming that this was the case, and only after written approval by the Local Planning Authority shall the development be commissioned and/or occupied.

Reason: To ensure that any ground and water contamination is identified and adequately assessed, and that remediation works are adequately carried out, to safeguard the environment and to ensure that the development is suitable for the proposed use in accordance with Policy 8 of the Core Strategy 2008 and the National Planning Policy Framework (2019).

26. Piling

No piling shall take place until a piling method statement (detailing the depth and type of piling to be undertaken and the methodology by which such piling will be carried out, including measures to prevent and minimise the potential for damage to subsurface sewerage infrastructure, and the programme for the works) has been submitted to and approved in writing by the local planning authority in consultation with Thames Water. Any piling must be undertaken in accordance with the terms of the approved piling method statement.

Reason: To prevent any potential to impact on local underground water and sewerage utility infrastructure ensure that any ground and water contamination is identified and adequately assessed, to safeguard the environment and to ensure that the development is suitable for the proposed use in accordance with Policies 8 and 9 of the Core Strategy 2008 and the National Planning Policy Framework (2019).

27. Foul drainage

Construction works shall not commence until a drainage strategy detailing any on and/or off site foul drainage works, has been submitted to and approved by, the local planning authority in consultation with the sewerage undertaker. No discharge of foul water from the site shall be accepted into the public system until the drainage works referred to in the strategy have been completed.

Reason: The development may lead to sewage flooding; to ensure that sufficient capacity is made available to cope with the new development; and in order to avoid adverse environmental impact upon the community in accordance with Policies 8 and 9 of the Core Strategy 2008 and the National Planning Policy Framework (2019).

28. Impact Studies

Prior to commencement of development details of Impact Studies which identifies the existing water supply infrastructure in order to determine the magnitude of any new additional capacity required in the system and the location of a suitable connection point shall be submitted to and approved in writing by the Local Planning Authority.

Reason: To ensure that the water supply infrastructure has sufficient capacity to cope with the additional demand in accordance with Policies 8 and 9 of the Core Strategy 2008 and the National Planning Policy Framework (2019).

29. Car Parking Permit Restrictions

No occupier of the residential units with the exception of disabled persons that are registered blue badge holders, shall be entitled to a car parking permit or retain such a permit for any Controlled Parking Zone (CPZ) in the Borough. If such a permit is issued to a resident of the development, it shall be surrendered to the Council within seven days of receipt.

Reason: In order to ensure that the development does not harm the existing amenities of the occupiers of neighbouring residential properties by adding to the level of on-street car parking stress in the area, in accordance with Policy T2 of the Adopted Local Plan (2004), Policies 7 and 8 of the Core Strategy 2008, the guidance contained in the Council's Developer's Guide Part 3 (2008) and the National Planning Policy Framework (2019).

30. Car Park Permit Restriction Scheme

The development hereby permitted shall not be occupied until such time as a scheme has been submitted to and approved in writing by the local planning authority to ensure that all occupiers of the residential units as labelled on approved plans listed in condition 2 other than those with disabilities who are registered blue badge holders, have no entitlement to parking permits from the Council and to ensure that occupiers are informed, prior to occupation, of such restriction. The development shall not be occupied otherwise than in accordance with the approved scheme.

Reason: In order that the prospective occupiers of the new residential units are made aware of the fact that they will not be entitled to an on-street car parking permit, in the interests of the proper management of parking, and to ensure that the development does not harm the existing amenities of the occupiers of neighbouring residential properties by adding to the level of on-street car parking stress in the area, in accordance with Policy T2 of the Adopted Local Plan (2004), Policies 7 and 8 of the Core Strategy 2008, the guidance contained in the Council's Developer's Guide Part 3 (2008) and the National Planning Policy Framework (2019).

31. Wheelchair User Dwellings and Adaptable Dwellings

A minimum of 5 units as labelled as Wheelchair Adaptable Homes as shown on the plans hereby approved shall be provided to Building Regulation requirement M4 (3) as Wheelchair User Dwellings.

All other remaining flats as shown on the plans hereby approved shall be provided to meet Building Regulation requirement M4 (2) 'accessible and adaptable dwellings'.

Reason: To ensure that the development provides for the changing circumstances of occupiers and responds to the needs of people with disabilities, in accordance with Policy EN1 of The Adopted Local Plan for Slough 2004, Core Policies 3, 4 and 8 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2019).

32. Level Accesses/Thresholds

The ground floor entrance doors to the Development shall not be less than 1 metre wide and the threshold shall be at the same level to the paths fronting the entrances to ensure level access. Level thresholds shall be provided throughout the development between the residential units and the external amenity/balconies and the main lobbies.

Reason: In order to ensure the development provides ease of access for all users, in accordance with Policy EN1 of The Adopted Local Plan for Slough 2004, Core Policy 8 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2019).

33. No Telecommunications Equipment

Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any Order revoking or re-enacting that

Order with or without modification), no aerials, antennae, satellite dishes or related telecommunications equipment shall be erected on any part of the development hereby permitted, without planning permission first being obtained.

Reason: To ensure that the visual impact of telecommunication equipment can be considered in accordance with Policy EN1 of The Adopted Local Plan for Slough 2004, Core Policy 8 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2019).

34. No other alterations to the buildings

No alterations shall be carried out to the external appearance of the development hereby approved, including the installation of air conditioning units, water tanks, ventilation fans or extraction equipment, not shown on the approved drawings.

Reason: To ensure a satisfactory external appearance and prevent harm to the street scene, and to safeguard the amenities of neighbouring residential occupiers, in accordance with Policy EN1 of The Adopted Local Plan for Slough 2004, Core Policy 8 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance contained in the Council's Developer's Guide Part 4 (2008) and the National Planning Policy Framework (2019).

35. Bat and Bird Boxes

Prior to occupation of any part of the development, details of a scheme for Bird Bat and Boxes shall be submitted to and approved in writing by the local planning authority. The scheme shall detail the location, design, size and material of the bat and bird boxes and elevations and plans shall be provided to identify the bat and bird boxes to the satisfaction of the local planning authority. The development shall be carried out in accordance with the scheme prior to any occupation of the development and shall be permanently retained and maintained thereafter.

REASON: To safeguard habitats for birds and to deliver net gains in biodiversity in accordance with the National Planning Policy Framework (2019).

36. Fire Safety

The development shall be implemented in accordance with the design guidance and measures set out in Chapters 3: Means of escape; 4 Fire safety systems, 5 Internal fire spread and fire resistance, 6 External fire spread, 7 Fire Service Access and 8 Management set out in BB7 Fire Strategy by Stephen Davy Peter Smith Architects (Dated December 2018).

Reason: To ensure that the Development contributes to the minimisation of potential fire risk in accordance with National Planning Policy Framework (2019).

Informatives

1. The decision notice issued on 04.03.2020 was issued in error. This was recalled and replaced with this decision notice dated 25.03.2020 which identifies the correct drawing numbers (for the first, ninth and tenth floor plans as stated in condition 2). This s73 will update the decision notice dated 25.03.2020.

Informative updated to reflect latest application.

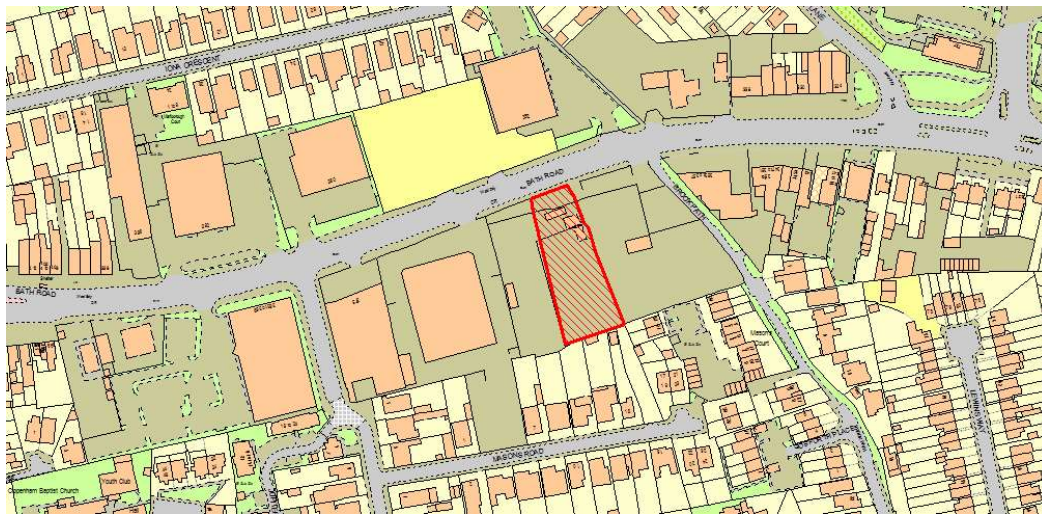
2. In accordance with paragraphs 38 and 39 of the National Planning Policy Framework (2019), Slough Borough Council takes a positive and proactive approach to development proposals and is focused on seeking solutions where possible and appropriate. Slough Borough Council works with applicants/agents in a positive and proactive manner by offering a pre-application advice service and updating applicants/agents of any issues that may arise in the processing of their application as appropriate and, where possible and appropriate, suggesting solutions. In this case, the applicant entered into a Planning Performance Agreement with the Local Planning Authority and was informed of the issues arising from the proposal and given the opportunity to submit amendments or provide additional information in order to address those issues prior to determination. The applicant responded by submitting revised plans and additional technical information which was considered to be acceptable.
3. The applicant is advised that the decision notice should be read alongside a s106 Legal Agreement which contains Planning Obligations entered into in connection with the planning permission.
4. This notice DOES NOT convey any consent that you may require for Building Regulations. If you are unsure whether you need Building Regulations approval and before you start any work please contact Building Control Services independently on (01753) 875810 to check whether they require an application.
5. The applicant is advised that an application for advertisement consent is required for any signage, adverts or shopfront fascia displays.
6. The applicant will need to apply to the Council's Local Land Charges on 01753 875039 or email to 0350SN&N@slough.gov.uk for street naming and/or numbering of the unit/s.
7. No water meters will be permitted within the public footway. The applicant will need to provide way leave to Thames Water Plc for installation of water meters within the site.
8. The development must be so designed and constructed to ensure that surface water from the development does not drain onto the highway or into the highway drainage system. The applicant is advised that if it is intended to use soakaways as the method of dealing with the disposal of surface water then the permission of the Environment Agency will be necessary.

9. The permission hereby granted shall not be construed as authority to obstruct the public highway by the erection of scaffolding, hoarding, skip or any other device or apparatus for which a licence must be sought from the Highway Authority.
10. The applicant must apply to the Highway Authority for the implementation of the works in the existing highway. The council at the expense of the applicant will carry out the required works.
11. The applicant will need to take the appropriate protective measures to ensure the highway and statutory undertakers apparatus are not damaged during the construction of the new unit/s.
12. During the construction phase of the development hereby permitted the developer is asked to ensure contractors are engaged without reliance upon working unusual hours on site nor reliance upon unusual practices that are likely to cause a nuisance to nearby residents or road users. In general no work should be carried out on the site outside the hours of 08.00 hours to 18.00 hours Mondays - Fridays, 08.00 hours - 13.00 hours on Saturdays and at no time on Sundays and Bank/Public Holidays. Car parking for construction workers and space for deliveries should be within the application site.
13. A Groundwater Risk Management Permit from Thames Water will be required for discharging groundwater into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. Thames Water would expect the developer to demonstrate what measures they will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 02035779483 or by emailing wwqriskmanagement@thameswater.co.uk. Application forms should be completed on line via www.thameswater.co.uk/wastewaterquality.
14. With regard to surface water drainage it is the responsibility of the developer to make proper provision for drainage to ground, water courses or a suitable sewer. In respect of surface water it is recommended that the applicant should ensure that storm flows are attenuated or regulated into the receiving public network through on or off site storage. When it is proposed to connect to a combined public sewer, the site drainage should be separate and combined at the final manhole nearest the boundary. Connections are not permitted for the removal of groundwater. Where the developer proposes to discharge to a public sewer, prior approval from Thames Water Developer Services will be required. The contact number is 0800 009 3921.
15. With regard to water supply it is the responsibility of the developer to ensure that there would be sufficient capacity for the future residents of the development. Prior approval from Thames Water Developer Services will be required. The contact number is 0800 009 3921.

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Registration Date:	22-Mar-2021	Application No:	P/00908/012
Officer:	William Docherty	Ward:	Cippenham Green
Applicant:	Mr. Simon Allen, Big Yellow Self Storage Company Limited	Application Type:	Major
		13 Week Date:	21 June 2021
Agent:	Stacey Swanson, DWD 6, New Bridge Street, London, EC4V 6AB		
Location:	361, Bath Road, Slough, SL1 5QA		
Proposal:	Construction of a part two, part three and part six storey building comprising a self storage facility (Use Class B8) together with vehicular access, service yard, parking, associated works and landscaping		

Recommendation: Delegate to Planning manager for Approval



P/00908/012

1.0 SUMMARY OF RECOMMENDATION

- 1.1 This application has been referred to the Planning Committee for consideration as the application is for a major development.
- 1.2 Having considered the relevant policies set out below, and comments that have been received from consultees, and all other relevant material considerations it is recommended the application be delegated to the Planning Manager for approval subject to the satisfactory agreement of conditions:

PART A: BACKGROUND

2.0 Proposal

- 2.1 The application is for full planning permission for the redevelopment of the site for the construction of a part two, part three and part six storey building comprising a self storage facility (Use Class B8) together with vehicular access, service yard, parking, associated works and landscaping.
- 2.2 The proposals include:
- Erection of a part 2, part 3 and part 6 storey building with a maximum ridge height of 20m from ground level.
 - 3,167 m² of self storage (B8) floorspace, increasing to 12,580m² including the 5 demountable mezzanine floors
 - Raised ground floor level, parking area and access to allow for the flow of flood water. Internal ground floor levels to be approximately 1m above external ground level to the front and 1.2m – 1.6m to the rear.
 - Access from Bath Road
 - Automatic gate
 - 14 Parking spaces including 2 accessible bays and one parent and child bay and 2 Electric Vehicle charging points
 - Extensive landscaping
 - Solar Panel Array on southern roof elevation (300m²)
 - 8 Cycle Spaces
 - Means of enclosure
 - Waste Storage area

3.0 **Application Site**

- 3.1 The site is located on the southern side of Bath Road and covers an area of approximately 0.79 hectares (1.96 acres). The site is bounded by residential uses (comprising of both houses and flats) to the south, a car dealership to the west with large commercial stores beyond this. Brook Path is to the east, which provides vehicular access south to Bower Way, with further commercial uses located to the east of Brook Path. To the north is Bath Road with a vacant site to the opposite side which is the subject of pre-application discussions.
- 3.2 The site is currently hard surfaced with some portable buildings on and was previously used by a vehicle rental business, with the associated parking of vehicles. The site has been used for vehicle sales and rentals for over 15 years. Vehicular access to the site is gained via Bath Road and there are currently three dropped kerbs in front of the site.
- 3.3 The site is located within flood zone 3a and in an area susceptible to surface water flooding. The western side of the of the site is allocated within the Slough Proposals Map as being within the Bath Road Business Area and also notes the site as forming part of an Area of Change as set out in the Core Strategy and a Selected Key Location for Comprehensive Regeneration (SKL1) as set out in the Local Development Framework Site Allocations DPD. Land to the front and east of the site has been proposed for future road widening. The site forms part of the Trade Sales Site as noted on the Brownfield register.

4.0 **Relevant Site History**

- 4.1 P/00908/011 Screening opinion request for the construction of a part two, part three and part six storey building comprising a self storage facility (Use Class B8) together with vehicular access, service yard, parking, associated works and landscaping

Decision: EIA Not required 27/04/2021

- 4.2 P/03484/020 Change of use of land to a vehicle hire depot incorporating use of existing modular building adjacent to western boundary to be utilised as reception and sales cabin and removal of two modular buildings.

Approved: 10-Dec-2013

- 4.3 P/03484/018 Use of land for vehicle display / sales (sui generis) including siting of modular office building with decking, provision of staff and customer parking, access from bath road (part-retrospective).

Approved: 29-May-2013

- 4.4 P/00908/006 Erection of three storey office block (outline) Subsequent application to extend consent for a further three years approved (ref. P/00908/009) in 1997
Approved: 10/11/1993

- 4.5 P/00908/007 Use of part of the site as a workshop
Approved: 22/03/1993

- 4.6 P/00908/008 Erection of car showroom and workshop
Withdrawn: 02/06/1998

20 Mason's Court:

P/00545/019 Variation of condition no. 4 of planning consent p/00545/016 dated 20/08/2009 to allow windows to be added.

Approved: 18-Oct-2011

- 4.7 Pre- Application Advice issued October 2020

Development considered:

Construction of a part three and part five storey building comprising a self storage facility (Use Class B8) and part four and part five storey flexi office accommodation (Use Class B1) and the construction of 3 blocks of three to five storeys to provide 49 self-contained flats (Use Class C3) together with vehicular access, service yard, parking, associated works and landscaping.

Advice Provided:

The residential element of this advice has been removed from the proposed scheme and therefore this is not included below. The advice recommendations in relation the proposed storage unit are noted below:

- Concerns with impacts of a more intensive B8 storage use; LPA happy to review a method of restricting this use
- Concerns with impacts of storage unit on highways and living conditions for existing and any future residential development with unit being used 24 hours a day
- Principle of any development on this site will be led by the flooding issues
- Potential Overbearing impact on properties to the rear
- Close positioning for the storage unit to the rear gardens of Mason's Road result in poor visual amenity for residents.
- Potential sterilisation of future residential development within SKL1

resulting from the storage facility being close to the western boundary

The advice has been taken into account in the submission with alterations made to the location of development, hours of use and proposed design.

5.0 **Neighbour Notification**

5.1 In accordance with Article 15 of The Town and Country Planning (Development Management Procedure, Listed Buildings and Environmental Impact Assessment) (England) (Coronavirus) (Amendment) Regulations 2020 six site notices were displayed – 2 on Bath Road to the front of the site, 1 on Brook Path, 1 on the fence between 19 and 20 Mason's Court and 2 on Mason's Road - each dated 31/03/2021. The application was advertised as a major application in the 30/04/2021 edition of The Slough Express.

5.2 No responses have been received.

6.0 **Consultations**

6.1 **Local Highway Authority:**

Introduction

This document provides Slough Borough Council's consultation response regarding Highways and Transport for application P/09811/0012 for a Big Yellow Storage Facility measuring 12,580sqm at 361 Bath Road, Slough. A Transport Statement (TS) has been prepared by i-Transport. The Proposed Ground Floor Site Plan is shown on Mountford Piggot Drawing No. 2102-P01-H (Dated: 08/02/2021).

Preapplication advice was provided by Highways Officers during August 2020. Initial comments were provided on 12th May 2021. i-Transport provided Technical Note ITL14431-005A-TN on 15th June in response. An amended proposed Site Plan has been submitted displaying a 2 metre wide strip along the site's boundary with Brook Path for future dedication as public highway, a new lighting column on Brook Path and dropped kerb and tactile paving.

Application Description

The site is currently occupied by a vehicle hire company (Van Minster, specialising in van hire) and comprises surface level vehicle storage with some small temporary buildings. The proposed building would have permanent ground floor space of 3,167sqm and 5 demountable mezzanine floors, enabling self storage of up to 12,580sqm. The site has three existing vehicular access/ along the site frontage, taken from Bath Road.

Access

It is proposed to rationalise the existing three accesses into one main access, in accordance with SBC's spatial strategy. The existing bellmouth for Brook Path and Van Minster are poorly separated at present and will be separated into two separate bellmouth junctions. The applicant will be required to enter into a Section 278 agreement for the detailed design. The applicant has also offered dropped kerbs and tactile paving for the crossings of the site access and Brook Path, as show on i-Transport Drawing ITL14431-GA-026-D, dated 11th June 2021.

In addition, the applicant has offered to provide a 2 metre wide strip of land on Brook Path along the site's eastern boundary and provide a lighting column, at the request of SBC Highways and Transport. The 2 metre strip and lighting column are shown on Mountford Piggot Drawing No. 2102-P01-H, dated 12.02.21.

A separate legal agreement for the 2 metre strip of land would enable SBC to dedicate the land as public highway in future. SBC could use the land to widen and further upgrade Brook Path in future, should further funding become available from other funding sources or developments.

i-Transport Drawing ITL14431-GA-026-D provides visibility splays from the proposed site access, with a splay of 120m to the left and 45m to the right. Visibility splays of 2.4m x 120m are required for a 40mph speed limit by the Design Manual for Roads and Bridges (DMRB), therefore the existing right-hand visibility splay is sub-standard. SBC accept the existing visibility splays shown on i-Transport Drawing ITL14431-GA-026-D on the basis that the proposed development is expected to reduce the number of vehicles arriving/departing the site compared to the site's previous use as a car hire centre.

It is proposed that the service yard will be raised and accessed by a 1:18 ramp. The ramp is required because the site sits within Flood Zone 3 and the site will be raised 660mm due to a requirement for the site to be raised at least 600mm above the highest flood zone level. The proposed access ramp is considered acceptable by SBC Highways and Transport.

The TS includes collision data obtained from AccStats for the 5 year period between 01/01/2014 – 31/12/2018, which is at least 2 years out of date.

At the request of SBC Highways and Transport, i-Transport have obtained collision data for the most recently available 5-year period. Collision data was provided for the A4 Bath Road and it's junctions with Elmshott Lane, Station Road, the Retail Park Signal Junction, Stowe Road and Burnham Lane. A total of 10 collisions resulting in injury were recorded within the study area, comprising nine slight and one serious injury accidents. No fatal accidents were recorded.

No accidents were recorded at the site access junction in the most recent 5-year period. The proposed development is unlikely to worsen the existing collision rate given the proposed use is forecast to generate fewer vehicle trips than the previous land use.

Therefore SBC Highways and Transport have no objection to the planning application based upon the collision recorded in the area.

Access by Sustainable Travel Modes

The site benefits from reasonable opportunities for employees to travel to work by sustainable travel modes.

The site benefits from being located 220m from bus stops on the A4 Bath Road, which functions as a key bus route within Slough. There are further stops on Bower Way, which is located 420m from the site. The No. 4 service provides 2 buses per hour between Maidenhead, Slough and Heathrow between Monday and Friday. The No. 5 service provides 2 buses per hour between Slough and Cippenham between Monday and Friday.

A Study into Public Transport Access Level (PTAL) in Slough was undertaken by Atkins in 2018. This section was given a rating of '1b' for Public Transport Access Level, which denotes a low level of public transport accessibility.

Burnham Station is located approximately 780m from the site and offers 4 trains per hour to London Paddington and Reading during the peak hours.

Trip Generation

The net change in the site's trip generation has been forecast by subtracting the existing trip generation from the forecast trip generation. The existing trip generation of the site was estimated by using data from TRICS, the national trip generation database.

For existing Trip Generation, the TS estimated that the existing vehicle hire company generates around 24 two-way vehicle trips in the morning peak hour and 35 two-way vehicle trips in the evening peak hour. Across a 12-hour day, it is estimated to have generated some 400 two-way vehicle trips.

For the proposed use, the TS forecasts Trip Generation using data collected from an existing Big Yellow store totalling 8,623sqm in New Cross, London. The trip rates per square metre derived from this survey site have been used to forecast 4 two-way trips during the AM Peak Hour and 7 two-way trips during the PM Peak Hour, for a facility comprising 12,580sqm. This is equivalent to one trip every 9 to 15 minutes.

The TS forecasts that the development will generate 20 fewer two-way vehicle trips during the AM Peak Hour and 28 fewer two-way vehicle trips in the PM Peak Hour. Over a full day, the development is forecast to generate 209 fewer two-way vehicle trips.

Furthermore the peak hours of operation for self-storage facilities do not coincide with traditional highway network peak hours and the peak for the Big Yellow Store at New Cross was between 1400 – 1500.

SBC Highways and Transport accept the forecast trip generation presented in the TS for the proposed development. No objection is raised based on the proposed development's impact on highway capacity.

Parking

14 Car Parking Spaces are proposed, including one disabled space and one parent and child space.

At the request of SBC, the applicant has increased the size of 5 parking spaces to dimensions of 2.7 metres x 5.2 metres (with an additional 1.2m hatched area).

The revised parking layout is displayed on Drawing No. ITL14431-GA-026-D. Three loading bays are also provided within the servicing yard for larger vehicles. Swept path analysis has been provided on Drawing No. ITL14431-GA-021-D and Drawing No. ITL14431-GA-022-D which demonstrates the three loading bays can accommodate 10m long vans or a 16.5m long articulated lorry in the unlikely event this is required.

The TS has provided an estimate of parking demand based upon survey data of a big yellow storage facility measuring 8,623sqm in New Cross. The TS states that at the New Cross store there was a maximum parking accumulation of 11 vehicles during any 15-minute period across the whole survey day.

SBC Highways and Transport accept the 14 proposed parking spaces.

Cycle Parking

Four Sheffield stands providing eight parking spaces are proposed to serve the site. This would fall below the number required by the SBC Parking Standards; however the nature of the storage facilities as a use does not attract many cycle trips given the need to transport items to/from the storage units. The cycle parking provided is likely to be used solely by staff of the proposed unit.

SBC Highways and Transport accept the proposed number of Sheffield stands / cycle parking spaces.

Servicing and Refuse Collection

The TS states HGV trips will be infrequent; although the turning yard has been designed to allow sufficient turning space for a 16.5m articulated vehicle for instances when an articulated vehicle needs to access the site. Swept paths have been provided on Drawing No. ITL14431-GA-021, dated 30/11/2020 which demonstrates that an Articulated Vehicle measuring 16.48m in length can ingress and egress the site. Drawing No. ITL14431-GA-029, dated 16/03/2021 demonstrates that a Phoenix 2 Duo Recycler Refuse vehicle measuring 11.2m in length can ingress and egress the site to complete refuse collection. The swept paths are accepted by SBC.

Summary and Conclusions

I confirm that I have no objection to this application from a transport and highway perspective. I recommend the inclusion of condition(s) informative(s) as part of any consent that you may issue.

6.2 Thames Water:

No objections subject to informatives

Waste Comments

Thames Water would advise that with regard to FOUL WATER sewerage network infrastructure capacity, we would not have any objection to the above planning application, based on the information provided.

Thames Water would advise that with regard to SURFACE WATER network infrastructure capacity, we would not have any objection to the above planning application, based on the information provided.

Water Comments

If you are planning on using mains water for construction purposes, it's important you let Thames Water know before you start using it, to avoid potential fines for improper usage. More information and how to apply can be found online at thameswater.co.uk/buildingwater.

Supplementary Comments

Comments are based on the assumption that Foul and Surface water will drain by gravity (Surface water attenuated to 2l/s) to manhole connections to the east of the site (either SU94805903, SU94805902 or SU94814001 for Foul.... and either SU94814050 or SU94805952 for Surface water).

6.3 Lead Local Flood Authority (Hampshire CC)

The LLFA have reviewed the following:

- Ground Floor Site Plan As Proposed Drawing No. 2102-P01-B Rev B Date 01.03.21 Prepared by Mountford Pigott
- Topographic Survey Drawing No. 17042/T/02-02 B Date April 2018 Prepared by EDI Surveys Ltd
- Thames Water Asset Location Plan
- Drainage Strategy and Maintenance Statement Document Reference No. 12904-CRH-ZZ-XX-RP-C-0002_DS P2.docx

No objections to the proposals.

6.4 Environment Agency

Initial Objection (Consultation reply 1) – 25th May 2021

The submitted FRA does not comply with the requirements of paragraph 163 of the National Planning Policy Framework (NPPF) and for site-specific flood risk assessments, as set out in paragraphs 30 to 32 of the Flood Risk and Coastal Change section of the planning practice guidance. The FRA does not therefore adequately assess the flood risks posed by the development. In particular, the FRA fails to:

- take the impacts of climate change into account
- Flood risk mitigation measures to address flood risk for the lifetime of the development included in the design are inadequate because they will not make the development resilient to the flood levels.
Consequently the development proposes inadequate:
 - Flood storage compensation:
 - Voids
 - Volumetric compensation scheme

This objection is also supported by Core Policy 8 of the Slough Local Development Framework Core Strategy. 2006-2026 (adopted December 2008).

Overcoming our objection

We are satisfied with the elevated ground floor level and the modelling that this is based on, however we would like to see more detail regarding the design of voids that will go below the ground floor. The void openings should be a minimum of one metre wide. Their minimum height, for the full one metre width of the opening, should be up to the predicted depth of flooding (including climate change and freeboard allowance), extending from the existing ground level. There should be one opening in every five metre length of wall on all sides.

Voids should be open and maintained as such in perpetuity. If the void openings are a security risk, then vertical steel bars placed at 0.1m centres can be installed. To overcome our objection, the applicant should submit a revised FRA which addresses these points. The applicant should provide a design or written description that the building will meet these specifications.

In order to provide a full understanding of the flood compensation scheme, the applicant should provide information on the current built footprint within the site, and how this compares to the proposed built footprint of the self storage facility. If this cannot be achieved, we are likely to maintain our objection. Please re-consult us on any revised FRA submitted and we'll respond within 21 days of receiving it.

Fencing design

Walls and fences can have a significant impact on the flow and storage of flood water, especially if they are constructed across a flood flow route. This can lead to higher levels of flood water on the upstream side of the fence or wall which will potentially increase the flood risk to nearby areas. Therefore walls and fences should be permeable to flood water.

We recommend the use of post and rail fencing, hit and miss fencing (vertical slats fixed alternately on each side of horizontal posts) or hedging. If a solid wall is proposed there must be openings below the 1% annual probability (1 in 100) flood level with an appropriate allowance for climate change to allow the movement of flood water. The openings should be at least 1 metre wide by the depth of flooding and there should be one opening in every 5-metre length of wall.

Second Objection (Consultation reply 2) – 5th August 2021

In the absence of an acceptable Flood Risk Assessment (FRA) we maintain our objection to this application and recommend that planning permission is refused.

Reasons: The submitted FRA does not comply with the requirements of paragraph 167 of the National Planning Policy Framework (NPPF) and for site-specific flood risk assessments, as set out in paragraphs 30 to 32 of the Flood Risk and Coastal Change section of the planning practice guidance. The FRA does not therefore adequately assess the flood risks posed by the development.

In particular, the FRA fails to take the impacts of climate change into account:

- Flood risk mitigation measures to address flood risk for the lifetime of the development included in the design are inadequate because they will not make the development resilient to the flood levels.
Consequently the development proposes inadequate:
 - Flood storage compensation: Voids

This objection is also supported by Core Policy 8 of the Slough Local Development Framework Core Strategy. 2006-2026 (adopted December 2008).

Overcoming our objection

We appreciate the additional information provided which satisfies our concerns over the fencing and floodplain compensation scheme.

However on review of the image and description of the proposed voids under the building, we do not find that it meets the required specifications for an appropriate void as set out in our previous objection. We have no problem with the mesh screen that will provide security but allow the free flow of water. However, the soffit of the void needs to be set at a minimum height of the predicted flood extent level plus climate change and there needs to be 1 metre wide openings for every 5m length of wall under this soffit. If the soffit of the void is below the castellated beam, then the beam needs to be raised above the flood extent level plus climate change. If the soffit of the void is above the beam, then there need to be adequate openings of a minimum of 1 metre width every 5 metres throughout the length of the beam.

In order to overcome the objection, the applicant will need to raise the beam so that the underside is above the flood extent level plus climate change, or change the design of the openings in the castellated beam so that there are 1 metre wide openings in every 5 metres of wall on all sides.

To overcome our objection, the applicant should submit a revised FRA which addresses these points. The applicant should provide a design or written description that the building will meet these specifications. If this cannot be achieved, we are likely to maintain our objection.

Removal of objection (Consultation reply 3) – 29th September 2021

We have reviewed the following documents:

- Response to objection dated August 2021, reference: FGAfga12904-20210809-EA P3

- Fence and Void Details as Proposed 2102-P10-B (dated 02/08/2021)

In the response to the Environment Agency objection letter, it is noted that the beam has been redesigned so the castellation is removed and is therefore clear from obstruction. The lower edge of the beam is also raised above the 1% AEP plus 35% climate change flood level. The newly submitted drawing of the void demonstrates that the beam is now completely open without holes.

The submitted information satisfactorily addresses our earlier concerns.

Subject to the condition, number 31, below, we therefore withdraw our previous objection.

Environment Agency position

The proposed development will only meet the National Planning Policy

Framework's requirements in relation to flood risk if the following planning condition is included.

6.5 SBC Scientific Officer

No objections subject to conditions

The scientific officer reviewed the "Geotechnical and Geoenvironmental Desktop Study" (Project Number: 12904), dated March 2021 and prepared by Campbell Reith Hill LLP.

The following comments were made:

- The preliminary assessment identified the site to have a medium to high sensitivity for hydrogeology, and low sensitivity for human health receptors.
- Potential sources of contaminations have been identified that required further investigation and assessment in order to correctly quantify the risks associated with the development and to future site users.
- The overall site risk in relation to potential contamination issues has been identified as low to moderate. Thus, further investigation is required.

6.6 SBC Landscape/Arboricultural Officer

Scheme will retain established trees and provide an extensive new landscape that will provide considerable biodiversity net gains. No objection subject to conditions

There are no TPOs on the site. The submitted arboricultural information is sufficient to ensure established trees growing around the development will not be harmed during the construction phases. The implementation of the Tree Protection Plan contained within the submitted; Tree Survey Report and Arboricultural Impact Assessment JSL3903_770 Tree Survey and Arboricultural Impact Assessment B 09 March 2021 should be a condition of any approval.

The submitted Biodiversity Net Gain Assessment provides of a significant uplift in biodiversity provided that the outline landscape scheme provided within the BIG YELLOW STORAGE : SLOUGH WEST Landscape Design Report 15.03.2021 ref: LN00637. P001 is provided.

6.7 SBC Noise and Air Quality Officer

Air Quality Comments

An air quality assessment, prepared by RPS Group, has been submitted with this application. The proposed development includes 14 parking spaces and 3 loading bays. The transport statement indicates that the proposed development will result in 191 AADT, and 1 HGV AADT. This is under the threshold specified

within the LES, EPUK and IAQM, therefore the proposal is not expected to cause an operational air quality issue. It is also noted that this does not consider the loss of vehicle movements (previous use as vehicle hire depot), therefore operational impact will be very low.

Air quality impacts during construction have also been considered. The assessment determines that the proposal will cause a medium dust impact during the construction phase, however this can be mitigated through the implementation of the CEMP, which has also been reviewed as part of this application. Comments on the CEMP are provided below:

- The CEMP states that non-road mobile machinery (NRMM) will meet stage IIIA of EU Directive 97/68/EC as a minimum. In line with the LES, controls must comply with table 10 of the LES technical report, which specifies the requirement to meet Stage IIIB of the Directive as a minimum.
- The LES also requires all construction vehicles to meet a minimum Euro 6/VI Emission Standard. Detail of this has not been supplied and will require confirmation.
- A construction traffic route plan has been provided however multiple routes are shown. It is requested that traffic access is encouraged to use Junction 7 of the M4 to avoid AQMA 3 and AQMA 4.
- The CEMP states that a Dust Management Plan will be provided, which contains details of PM10 monitoring which is to be agreed with the authority. This will be required via condition.
- Some of the mitigation listed in Section 6 of the air quality assessment has not been replicated within the CEMP. This detail should be included.

The overall impact of the scheme on air quality is minor. As such, the scheme requires the integration of Type 1 mitigation measures, contained within the LES Planning Guidance, in addition to the points highlighted above.

Mitigation Requirements

- Electric vehicle re-charging infrastructure should be provided in line with table 7 of the LES Technical Report. As the scheme includes 14 parking spaces, 2 must have access to electric charging infrastructure to meet the 10% requirement. As two fast chargers are proposed as part of the proposal, this requirement has been met.
- All heating systems shall meet the emission standards laid out in Table 7 of the LES Technical Report.

Environmental Noise Comments

An environmental noise assessment has been submitted in support of this application, prepared by Sharps Redmore.

As previously arranged with SBC, the baseline noise level has been informed by nearby noise assessments, as the monitoring period would have coincided

with the pandemic and would not have been representative. The noise assessments which have been considered were for developments on Bath Road, including 163, 225, 227, and 392 Bath Road. The background noise levels chosen for the purpose of this assessment are 48dB and 42dB during the day and night, respectively. As the locations where the background noise levels were collected were partially screened from traffic noise sources and this development is more exposed, this follows a conservative approach. The Council will require a verification survey to be completed once traffic levels have returned to normal, to confirm the background noise level at the nearest sensitive receptors.

The report states that typical operational hours are from 08:00-18:00, however access is permitted between 05:00-23:00. Vehicles accessing the site typically includes cars (47% packing, 20% emptying), courier vans (27%) and transit users (7%). Noise data of activity at other Big Yellow sites have been reported and collated with trip data provided by i-Transport, to calculate likely noise levels at 10m from the noise source during both the day and night. This has been calculated during the day as the busiest hour with 15 vehicles using the site, based on the percentage split provided above, and during the night based on 3 light goods vehicles accessing the site during a 15 minute period. This considers a range of noise sources including vehicle movements; movements of trolleys over rough ground; fork lift trucks (where used); door and boots slams; and speech.

The orientation of the proposed development provides noise screening to receptors in the south and existing buildings screen receptors to the east. This results in noise levels highest at receptors to the north of the site which do not benefit from screening, at 36dB LAeq1h and 42dB LAeq 15min during the day and night, respectively. This is equal to or lower than the background noise levels therefore noise impact is expected to be low.

Plant noise sources are expected to include small scale heating/cooling systems and extract fans. Plant details have not yet been specified therefore a planning condition will be imposed which limits future plant noise levels. The noise assessment suggests a more stringent noise condition than that which is required by the Council however as the developer wishes to obtain a credit under the BREEAM scheme, noise limits will be set to 5dB below background noise levels. The exact noise level which will act as the plant noise limit will be confirmed once the verification survey has been completed.

Noise caused by an increase in traffic on the road network has been considered and is expected to be <1dB as traffic increase will not exceed 2%.

Construction details have not been provided at this stage of the application, however a qualitative review of likely noise has been provided. Once full details of the construction phase are confirmed, an environmental noise assessment using the ABC method specified within BS 5228, should be produced and

submitted to the LPA, including details of mitigation required. This is required prior to commencement.

Summary

- The noise assessment indicates that the noise impact of the development at the nearest sensitive receptors will be low during both the day and night.
- As this assessment was based on noise data from other nearby assessments, it is required that a verification survey is completed once restrictions are lifted and traffic levels have returned to normal (currently scheduled to be post 21st June, subject to government guidelines), to confirm the background noise level at the nearest sensitive receptors, particularly from 05:00-07:00. This will be secured via condition.
- Plant details have not yet been confirmed, therefore plant noise will be limited to 5dB below background noise levels: The rating level of noise emitted by all fixed plant on the site shall not exceed 5dB below background noise levels, at 1m of the sensitive receptor façade. The measurement and assessment shall be made according to BS 4142:2014+A1.
- Once full details of the construction phase are confirmed, an environmental noise assessment using the ABC method specified within BS 5228, should be produced and submitted to the LPA, including details of mitigation required. This is required prior to commencement.

6.8 British Research Establishment Ltd (BRE) – Daylight and Sunlight Consultants

Impacts of the development on daylight and sunlight on surrounding residential uses and the neighbouring potential future development site would be acceptable.

6.9 Buckinghamshire Council

No objections.

6.10 Economic Development Officer

No response received for this application. Any comments received will be reported into the Amendment Sheet

PART B: PLANNING APPRAISAL

7.0 **Policy Background**

7.1 Slough Local Development Plan and the NPPF

Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires that applications for planning permission are determined in accordance with the development plan unless material considerations indicate otherwise. Annex 1 to the National Planning Policy Framework advises that due weight should be given to relevant policies in existing plans according to their degree of consistency with the Framework (the closer the policies in the plan to the policies in the Framework, the greater the weight that may be given). The revised version of the National Planning Policy Framework (NPPF) was published in July 2021.

The National Planning Policy Framework 2021 states that decision-makers at every level should seek to approve applications for sustainable development where possible and planning law requires that applications for planning permission be determined in accordance with the development plan, unless material considerations indicate otherwise.

Planning Officers have considered the revised National Planning Policy Framework 2021 which has been used together with other material planning considerations to assess this planning application.

National Planning Policy Framework:

Section 2: Achieving sustainable development

Section 4: Decision-making

Section 6: Building a strong, competitive economy

Section 9: Promoting sustainable transport

Section 11: Making effective use of land

Section 12: Achieving well-designed places

Section 14: Meeting the challenge of climate change, flooding and coastal change

Section 15: Conserving and enhancing the natural environment

The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008

Core Policy 1 – Spatial Strategy

Core Policy 5 – Employment

Core Policy 7 - Transport

Core Policy 8 – Sustainability and the Environment

Core Policy 9 – Natural, built and historic environment

Core Policy 10 – Infrastructure

Core Policy 12 – Community Safety

The Adopted Local Plan for Slough 2004 (Saved Policies)

EMP2 - Criteria for Business Developments

EMP12 - Remaining Existing Business Areas

EN1 – Standard of Design

EN3 – Landscaping Requirements
EN5 – Design and Crime Prevention
EN6 – Interference with Telecommunications Signals
EN34 – Utility Infrastructure
T2 – Parking Restraint
T8 – Cycle Network and Facilities
T9 – Bus Network and Facilities
T13 - Road Widening

Other Relevant Documents/Guidance

- Local Development Framework Site Allocations Development Plan Document
- Planning Practice Guidance (PPG)
- Slough Borough Council Developer's Guide Parts 1-4
- Slough Local Development Framework Proposals Map 2010
- ProPG: Planning & Noise: Professional Practice Guidance on Planning & Noise. New Residential Development. May 2017
- Emerging Local Plan for Slough 2016-2036:
 - Review of the Local Plan for Slough Issues and Options Consultation Document 16 January-27 February 2017(Dated 16/01/2017)
 - Emerging Preferred Spatial Strategy for the Local Plan for Slough 2013-2036 (Dated 01/11/2017)
 - Update on Emerging Preferred Spatial Strategy for the Local Plan for Slough 2013-2036 (Dated 21/02/2018)
 - Local Plan Spatial Strategy Overall Approach (Dated 29th July 2020)
- The National Design Guide (January 2021)
- Slough Low Emission Strategy 2018 – 2025
- DEFRA Technical Guidance TG (16). (Air quality).

7.2 The planning considerations for this proposal are:

- Principle of Development
- Impact on the character and appearance of the area
- Impact on amenity of neighbouring occupiers / uses
- Parking and highway safety
- Flood Risk and Drainage
- Economic impact
- Impact on biodiversity and ecology
- Air Quality
- Crime Prevention and anti-social behaviour
- Contaminated Land
- Sustainable design and construction
- Equalities
- Presumption in Favour of Sustainable Development

8.0 **Principle of development**

Land Use

- 8.1 Paragraph 81 of the National Planning Policy Framework seeks to create conditions in which businesses can invest, expand and adapt. Significant weight should be placed on the need to support economic growth and productivity, taking into account both local business needs and wider opportunities for development. Each area should be allowed to build on its strengths, counter any weaknesses and address the challenges of the future. Areas with high levels of productivity should be able to capitalise on their performance and potential.
- 8.2 Core Policy 5 (Employment) of the Core Strategy requires “major warehousing and distribution developments be located in the eastern part of the borough and in Existing Business Areas that have good access to the strategic road and rail network”.
- 8.3 Saved policy EMP2 (Criteria for Business developments) states that proposals for business developments will only be permitted if they comply with a set of criteria, including high quality design, no harm to neighbouring amenity, acceptable highways impacts and servicing.
- 8.4 Saved policy EMP12 (Remaining Existing Business Areas) states that a range of business developments will be permitted in the existing business areas which are not included in policies EMP1 and EMP6 to EMP11. B1(a) independent offices will only be permitted in accordance with the sequential test and where there would be no increase in the number of parking spaces provided.
- 8.5 The proposals would involve the construction of a storage unit, specifically for Big Yellow Storage, who have a number of sites throughout the UK. The floor area provided with the unit would be 3,167 m² of self storage (B8) floorspace, increasing to 12,580m², which takes into account the 5 demountable mezzanine floors. The demountable floors enable the quantum of floorspace to be increased which has been noted in the supporting documentation as being a practice employed for many years to maximise floorspace and is noted as being adopted in most of their sites.
- 8.6 The western side of the site is located within the Bath Road Existing Business Area, whilst not all of the site is located within the business area, the site context relates to the business area, with the Brook Path acting as a boundary, with businesses and properties to the east of Brook Path being smaller in scale. The site is also located within an Area of Change (Western End of Bath Road) as set out in the core strategy and in the SKL1, Selected Key Location for Comprehensive Regeneration, as set out in the Local Development Framework Site Allocations DPD. This allows for a possible relaxation in policy

for the loss of the existing business area in order to allow for residential or mixed use development.

- 8.7 Policy EMP12 states that a range of business developments will be permitted within the existing business areas not noted in policies EMP1 and EMP6-EMP11, this includes the subject site. The supporting text for the policy notes that B8 distribution and storage would be an acceptable use within these areas. The proposed use would therefore align with this policy.
- 8.8 Policy EMP2 requires business developments to meet a set of criteria, 2 of which relate to high quality design and visual and amenity impact are considered to be met and discussed further in the other sections of this report below. Other requirements including adequate servicing, highways impacts, appropriate landscaping, reduction in range of businesses and reduction in residential and shopping frontages are all considered to be complied with.
- 8.9 Core policy 5 notes that major warehousing and distribution developments, which it is considered this proposal would fall under, should be located in the eastern part of the Borough and in Existing Business Areas that have good access to the strategic road and rail network. Whilst the site is not located in the east of the Borough, the site is considered to have good access to the strategic road network, being directly located on the Bath Road (the A4), a strategic road, that provides connectivity and links Slough and Maidenhead, with the site being highly accessible to the M4; being positioned between Junction 6 and 7, and a short 3 minute drive to Junction 7 via the Bath Road and Huntercombe Spur. The site is also approximately 800m walking distance from Burnham Railway Station. It is considered that the proposed development would align with the objectives of this policy.
- 8.10 In looking at the objectives of SKL1 and the Major Area of Change, it is specifically noted that a possible relaxation of policy in this area, with residential and mixed use development potentially being permissible, despite the loss of the defined business area, could be allowed. In terms of the reasons noted for the SKL1 allocation, these state that alternative uses may need to be found for the Trade Sales car showrooms and sites in this part of the Bath Road providing the opportunity for the comprehensive redevelopment and regeneration of the area. It is noted in the site objectives that any residential or mixed use redevelopment of the site should improve the appearance of this important main road frontage; provides some family housing at the rear of the site; includes suitable amenity areas or gardens; minimises the number of access points onto the A4; provides for cycleways where appropriate; overcomes all flooding and drainage issues and protects the amenities of adjoining residential areas. It is noted that at pre-application stage a mixed use scheme including residential, flexi office use and storage was proposed, however this approach has not been pursued; it has been noted in the submission that the flood zone has caused difficulties with design and cost. Whilst a mixed use residential scheme has been considered on the site, given

the sites location within the flood zone 3a (The sequential test is discussed further below), a residential development, particularly consisting of family dwellings, would be difficult to accommodate and therefore the applicant has come forward with the current proposal for the site, which whilst not a mixed use or residential development fulfils the other policy context noted above. The proposal would also use a brownfield site and would make an effective use of an underutilised site within an existing business area.

8.11

Overall it is therefore considered that the proposed land use would be policy compliant and acceptable.

Sequential Test – Flooding

8.12

Section 14 of the National Planning Policy Framework sets out national policies for 'Meeting the challenge of climate change, flooding and coastal change.' Paragraph 159 states that 'Inappropriate development in areas at risk of flooding should be avoided by directing development away from areas at highest risk, but where development is necessary, making it safe without increasing flood risk elsewhere.'

8.13

Paragraph 161 clarifies the approach taken in this regard is to apply the sequential test to steer development away from areas at high risk and towards lower risk areas. If there are sequentially available sites at lower risk, then the exception test is applied.

8.14

Paragraph 162 states that development should not be allocated or permitted if there are reasonably available sites appropriate for the proposed development in areas with a lower risk of flooding.

8.15

The sequential test approach is required for major development within flood zones 2 and 3 where one has already not been carried out in allocation or other process. As noted above it seeks to direct development away from high flood risk areas, such as the subject site, to areas of lower flood risk. The planning practice guidance notes that: *'For individual planning applications where there has been no sequential testing of the allocations in the development plan, or where the use of the site being proposed is not in accordance with the development plan, the area to apply the Sequential Test across will be defined by local circumstances relating to the catchment area for the type of development proposed'*.

8.16

As part of the submission a Sequential Test was submitted however the search area for alternative sites was considered too narrow, being restricted to a narrow area along the Bath Road. Further to discussions with the applicant a new search area was agreed where the applicant and council would consider the availability of other sites suitable for the proposed development within. The new search area was a lot wider than the original and uses customer catchment areas of existing Big Yellow stores to define the search area, ruling

out areas which are covered sufficiently by their existing businesses. The customer information is collected by big yellow who use their customer postcodes to plot their position in relation to the particular site. This information was collated for the local regional stores a search area defined. The original search area excluded the east of the borough, which as identified in the 'Land Use' section above, core policy 5 notes as the preferred location for warehousing and distribution, and with areas such as Colnbrook and Poyle having large industrial areas it was questioned why this area had not been considered. It was noted that much of Slough is covered by an existing store located at 111 Whitby Road, SE1 3DR which already covers the majority of central Slough and the area to the east, with stores in Uxbridge and the projected customer base of the store under construction in Hounslow covering most of this area.

8.17

With the existing store in Slough it was noted that this store was leasehold, with all of the other stores in the location being freehold, which raised the question as to what the future of this store would be following the construction of the proposed store or whether expansion of this existing site had been considered. This was also of concern as if the existing store was to close, a wide area in the borough had been discounted from the sequential test search area because of its existence. It was confirmed by the applicant that the freehold of this site had been explored, however the site owners, SEGRO, would not consider the disposal of the freehold then or in the future due its position within the trading estate. It was also confirmed that had the store been available, given its level of visibility it would not be a site they would look to extend.

8.18

The loss of the existing store following the construction of the proposed was also addressed. It was noted that Big Yellow's lease of the existing Slough store expires in November 2034 and there is a tenant option to break the lease in November 2029; therefore the store lease does not expire for another 13 years, although there is an option in 8 years to break the lease, It has been also noted that Big Yellow also consider that there is enough business and demand for two stores to trade in Slough and they intend to trade both, with their position being that even if the existing Slough store was to close in 2029 or 2034 that they would most likely at that point look for another store in Central/ East Slough or consider whether other current catchments and available space can adequately cater for that demand. Officers challenged the applicant in respect of operating 2 stores in one town and were advised that it is not uncommon in their operating model as there are a number of towns and cities which have 2 or more Big Yellow stores including Guildford, Bristol, Liverpool and Sheffield, with a number of stores in London located within close proximity of one another. The location of the existing store in Slough has therefore been explored. As the search area identified area relates to the positioning of existing Big Yellow stores, it is considered appropriate that the development should only be used by Big Yellow going forward and therefore a condition requiring any future users of the building to acquire planning consent

before use of the building is to be imposed; this condition will also serve to protect the surrounding areas from more intensive B8 uses also.

8.19

The revised search area identifies land within Slough Borough Council, Royal Borough of Windsor and Maidenhead and the former South Bucks area. The search area is approximately 119 sq km and includes the Slough Industrial Estate. The search of the area utilised sites listed in the Slough Local Development Framework Site Allocations DPD, the Regulation 18 New Local Plan consultation document, the South Bucks Local Plan and the Royal Borough of Windsor and Maidenhead Local Plan Submission Version. The search also included planning applications using all three local planning authority's website search functions and to identify any windfall sites, a search for sites that are available on the open market and that are listed within the Costar Focus and EGi Property Link property databases. Within the search area only sites between 0.3 – 0.8 hectares (larger sites were considered where there may be potential to divide up the site) were considered and sites suitable for development for a self storage facility with associated yard and parking. The purpose of the parameters was to ensure that the site would ultimately be capable of providing the type and character of development required by Big Yellow and which is similar to the application scheme, it is noted in the PPG that *'When applying the Sequential Test, a pragmatic approach on the availability of alternatives should be taken.'*

8.20

The search identified 23 sites 10 in Slough, 10 in Windsor and Maidenhead and 3 in South Bucks meeting the criteria within the search area. Appendix 1 at the end of this report highlights the sites identified and a summary of the reasons why these have not been deemed either suitable or available for the development in question. Of the identified sites the majority of were classified as not being available for purchase, with some being developed already and others being pursued by other parties, either for employment/ business uses or residential development. Of the 23 sites identified there were 5 sites that were available or where part of the site was available; 4 of these are available to let and therefore unlikely to be suitable for the proposal's needs and 1 is available freehold. Of these 4 sites, none were considered to be suitable for a self storage facility because it is a site in the Green Belt not suitable for development; the location is not suitable; there is not good visibility; the building area or use or layout is unsuitable; and/ or the shape of the site is not suitable. Therefore following the sequential testing it has been concluded that there are no reasonable available alternative sites in a lower flood risk zone to accommodate the application within the agreed search area.

Conclusion on Principle of Development

8.21

In summary it is considered that the land use proposed is policy compliant and the revised sequential test, has been deemed acceptable and concludes that there are no reasonable available alternative sites in a lower flood risk zone to accommodate the proposal. It is therefore considered that the principle of the

use is acceptable.

9.0 **Design and impact on the character and appearance of the area**

9.1 Saved policies EN1, EN3 and EMP2 and Core Policy 8 of the Core Strategy require development to be of a high standard of design which respects, is compatible with and/or improves and the character and appearance of the surrounding area. Chapter 12 of the National Planning Policy Framework states “the creation of high quality, beautiful and sustainable buildings and places is fundamental to what the planning and development process should achieve”.

9.2 The Slough Local Development Framework Site Allocations DPD states that Selected Key Location 1 (SKL1) should be comprehensively planned in a way which improves the appearance of this important main road frontage.

9.3 Paragraph 134 of the National Planning Policy Framework requires development that is not well designed to be refused, especially where it fails to reflect local design policies and government guidance on design. Conversely, significant weight should be given to development which reflects local design policies and government guidance on design and / or outstanding or innovative designs which promote high levels of sustainability, or help raise the standard of design more generally in an area, so long as they fit in with the overall form and layout of their surroundings.

Brief Description

9.4 The proposed building is set in an L shape with the eastern section of the building being set back from Bath Road by approximately 40m running along the southern boundary at 3 storey level with an eaves height of 7 -7.5m from ground level with the 6 storey element located towards to the north west of the site projecting forwards being set back from the Bath Road by approximately 10m and having a ridge height of 20m and eaves height of 18.4m. The ground floor of the building will be raised 1m above ground level to the front and between 1.2-1.6m to the rear to allow for flood resilience measures and retain flood capacity. The footprint of the building is sizeable with a footprint of 3,167m² comprising mostly storage space with reception and circulation space. The upper floors are to be demountable mezzanine floors -which can be removed – for storage purposes. Landscaping is to be along all boundaries of the site with trees lining the front, rear and western boundaries.

Layout, Scale and massing

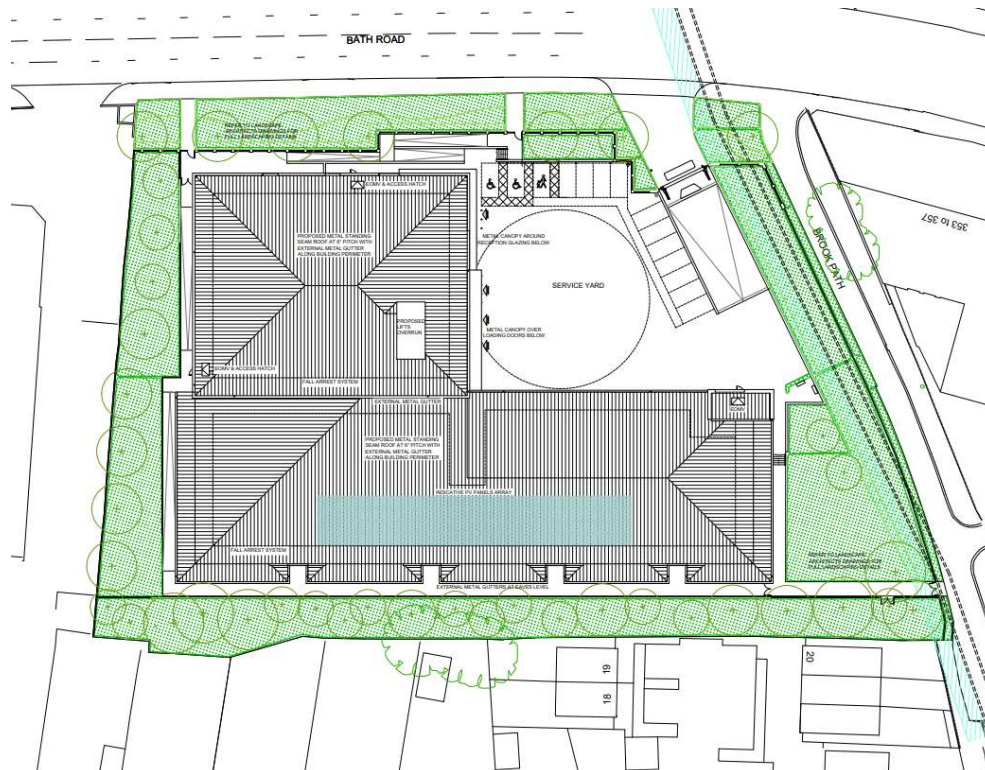


Figure 1 – Roof plan and site layout

9.5 As seen from figure 1 the proposed building would take up much of the site being a sizable structure, with 1 access from bath road to the north eastern side of the site leading to a raised service yard and parking area for 14 cars and vans. This plan also demonstrates the landscaping around the site with a particularly high level of trees located to the southern boundary, separating the building from the rear residential properties. The relationship of the building to Bath Road to the front of the site, Brook Path to the east, the residential dwellings to the south of the site, including 18 – 22 Masons Court to the south east and the rear gardens of 7-15 Masons Road to the south west and the Skoda and Seat garages to the west can also be seen above.

9.6 In the design process for the layout and massing it has noted that the massing has been set away from the southern boundary, pushing the building away from the residential gardens and buildings to the south by between 7.7m and 10m. The built form has been pushed away from the western boundary by between 8.7m and 11.1m, which is designed to ensure that if redevelopment proposals came forward on this site in the future, they will not be constrained or impacted by proposed building. The built form has also been pushed away from the lower part of the site in the south eastern corner to help in part to address issues with flooding around the site and to assist in keeping the proposed building away from some of the residential properties in Mount Court.



Figure 2 – Impression view from opposite side of Bath Road

9.7

The massing of the building is articulated into two distinct volumes, with the main volume closest to Bath Road comprising 6 levels to the northwest corner and the longer and lower volume to the southern elevation of the site. The proposed layout of the building allows for a landscaping strip to its front and despite the 6 storey element of the building being within 10m of public highway, it is considered that this set back is sufficient as to not be overbearing and overall is with the proposed soft landscaping would improve the pedestrian environment and not act as a constraint to possible future widening plans for the road. The building would also be approximately 4m forward of the building line of the existing Skoda/ Seat dealership, however with the 10m set back from the highways and 21m gap between these buildings it is not considered that this would be detrimental to the area.

9.8

The proposed scheme is noted as being at contrast to the existing site, which is essentially a hardstanding with no permanent buildings, and character of the area, which despite being commercial, Bath Road does not have buildings with height and scale as the proposed, being mostly 2 and 3 storeys in scale and a smaller footprint. Notwithstanding, it is noted that this is an Area for Major Change and the comprehensive regeneration provides an opportunity for a different scale of buildings which contrast with the existing character. Whilst the building would be at odds with the existing scale and massing in the area, it is not considered that any harm is significant or demonstrable to warrant the refusal of this application. Moreover in the medium - longer term it is expected that the character of the area along the Bath Road character will change with a likely greater scale of building/intensity of development.



Figure 3 – View from the north east side of Bath Road



Figure 4 – View from the north west on Bath Road

9.9 Looking at the site from the rear, it is acknowledged that the building will be a significant change to the existing situation, which with the site's lawful use as surface parking not having any visual impact on the properties to the rear. The building however has been designed to be articulated into 5 smaller blocks to assist in breaking up the bulk/mass of the lower section, with the mass being 2/3 storeys high with a mansard roof stepping up to the main mass of the building. This can be seen in figure 5 and 6 with the use of brick within these 5 smaller blocks also softening the appearance of this commercial building from the residential area to the rear.

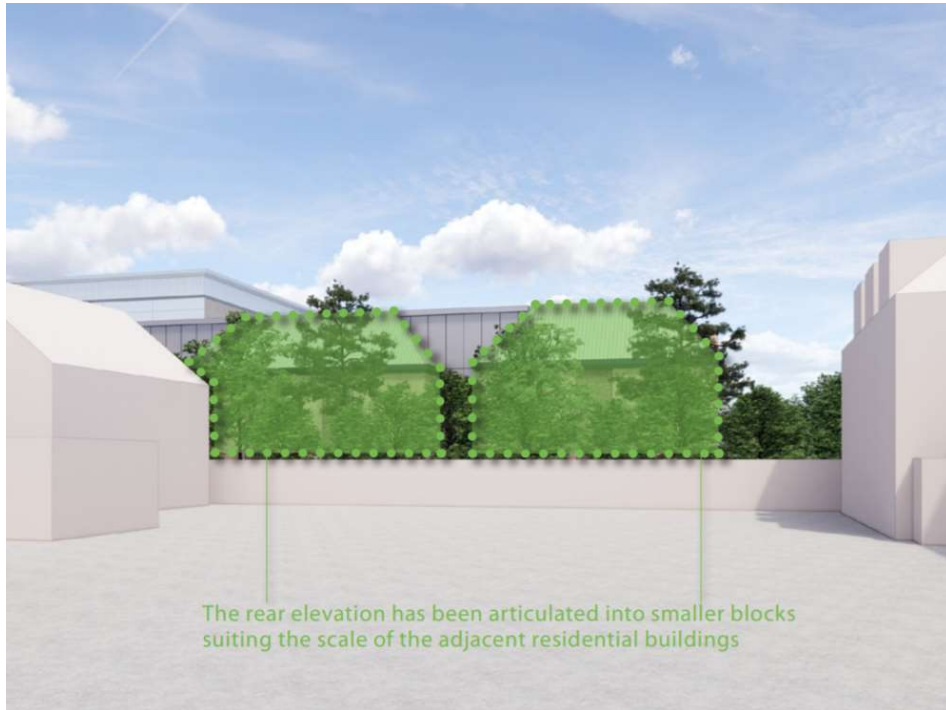


Figure 5 – View from Mount Court to the South East



Figure 6 – View from rear garden of dwelling on Mount Road to the south/south east

- 9.10 In summary, officers have considered the proposed siting, layout and scale of the commercial building on Bath Road and the residential character to the rear, south of the site. It is acknowledged that the building is of a scale and mass which is at a contrast to the existing context on the Bath Road, however; as noted above the site is located in an Area for Major Change and the comprehensive regeneration provides an opportunity for different scale buildings which contrast with the existing character. With this in mind it is considered the scale, siting and layout of the proposals are acceptable in terms

of the visual relationships and impact on the surrounding area.

Appearance

- 9.11 The front of the building will form part of the streetscene along Bath Road and is a gateway road into Slough Town Centre to the east and Maidenhead to the west. It will also be an important part of the setting for the future development to the north and west. The main 6 storey section of the building will use a blend of brick, glazing and coloured (Yellow and White) rain screen cladding, with the glazing sections with brick surrounds and a central cladding block of cladding. The glazing sections will reveal the workings of the building behind which are doors leading to storage areas. The roof section will be profiled metal cladding with a shallow pitch. The use of yellow across the building is consistent with the companies (Big Yellows) style and is present across different elements of the building.



Figure 7 - Proposed front, north, elevation of the building

- 9.12 To the eastern side of this part of the building, there is a section of glazing to the northern side framed in light brown brick which sits above the reception area, with a block of darker brick to the southern part, which is set above the loading bays which are surrounded by white metal composite panels. The lower eastern part of the frontage will have a light grey shallow pitched metal standing seam roof with silver grey metal composite insulated panels below with dark grey brickwork. To the eastern end of this section of the building has a block of light brown brickwork with a section of yellow and white cladding to round off this end of the building. The use of brick, glazing and the middle cladding section is considered to be an attractive approach to the building being of high quality design and materials creating an appropriate frontage onto Bath Road. The mix of materials on the eastern side of the building and glazing to front corner section again assists in creating an attractive frontage which can be seen particularly in figures 2 and 3 above. Furthermore, whilst landscaping is discussed in more detail in the next section of this report, the provision of planting to the front of the building will help soften the development and will provide visual benefits to its current barren commercial appearance.



Figure 8 – Eastern elevation, facing towards Brook Path

- 9.13 The rear section of the east elevation has a grey metal standing seam mansard roof with a mixture of light brown brickwork below and dark grey composite cladding, with security mesh on the lower part of the building to restrict access to the flood voids below the building. This section of the building will be between 14m and 21m from the Brook Path and the design and finish of this part of the building is considered acceptable.



Figure 9 – Southern, rear, elevation, facing towards Mason Court and Mason Road

- 9.14 To the southern, rear, elevation the building has 5 articulated blocks, which protrude 2.4m, with each block having a mansard style roof, with light brown brickwork with vertical darker brown brick strips running through to add detail. In the recessed sections of the 5 blocks are grey rainscreen cladding panels with dark grey brickwork to the lower section. A section of dark security mesh will run around the base of the building again to restrict access to the flood voids below the building. To the eastern side of the elevation the roof will pitch to a ridge, with the western side of the building rising to meet the 6 section of the building at the top of the 4th floor, where it meets to the rear wall. The rear wall of the 6 storey element is to be clad in silver grey metal composite insulated panels. The impact of this can be seen in figure 6.

- 9.15 The 5 articulated blocks are considered to assist with blending the scale of the

proposal with its residential surrounds to the rear. Whilst there is no getting away that the building is going to be a stark difference from the current situation from the rear of Masons Road and Masons Court, the current site being a hardstanding for vehicles, it is considered the use of brick and the shape of the blocks, mimicking a residential form, reduces the visual impact, with the sections of more commercial style cladding being used in the set backs and the rear of the 6 storey element. The 6 storey element is noted as being approximately 34m away from the rear residential boundaries. Furthermore, whilst the landscaping scheme is discussed in more detail in the next section, the provision of an extensive landscaping strip between the rear boundaries of the residential dwellings to the rear and the building will greatly assist in softening the impact of the scheme. Overall it is considered that proposal would have an acceptable visual impact on the areas to the rear of the site, with the proposed building having been informed by its context.



Figure 10 – Western elevation, facing towards Skoda/Seat dealership

9.16 The western elevation has a mix of dark brown brick and grey cladding to the 6 storey element with the rear section having a grey metal standing seam mansard roof with a mixture of light brown brickwork below and dark grey composite cladding, with security mesh on the lower part of the building to restrict access to the flood voids below the building. This elevation can be seen clearly from the Bath Road as indicated in figure 4. The elevation is considered an appropriate mix of brick and cladding with the cladding section set back 11m from the front elevation. In the existing context of Bath Road this faces onto the Skoda/Seat dealership which is a clad 2/3 storey commercial building. The use of different materials and the step down of the built form ensures an appropriate treatment to this elevation.

9.17 The proposals also include 2.3m high boundary treatments to the front, which consist of low level walling, with brick pillars with visually permeable fencing in between. Whilst this boundary treatment is high, the permeable fencing allows

views through into the site as not to be visually harmful and create a sense of enclosure or blank frontage and this would also be set over 5m away from the pavement behind soft landscaping. The fencing to the other sides and rear is proposed to be 2.4m black weld mesh fence which again would allow for views into the site whilst still providing security and is considered acceptable.

9.18 In summary it is considered that the appearance of the proposal would have an acceptable impact on Bath Road, Brook Path and the residential areas to the south. It is also considered that the design of the development would be compatible with future residential or mixed use development in the area, being of high quality design and materials. Due regard has also been had to the quality of the existing site which is a bleak barren area of hardstanding, with the proposals considered to enhance the site's appearance. Conditions will be recommended to ensure that the materials used are of high quality. Overall the appearance of the scheme is acceptable.

Hard and Soft Landscaping

9.20 The scheme includes a comprehensive landscaping scheme which includes planting to all sides of the building, including lawn border and tall hedge to the front, 1m high hedge along eastern boundary, wildflower planting to the east, shade tolerant shrubs to the southern and western boundary and extensive tree planting to around the site with approximately 40 trees proposed. The landscaping scheme can be seen in figure 11 with the tree planting scheme, without key, seen in figure 12, with each coloured circle representing a tree to be planted.

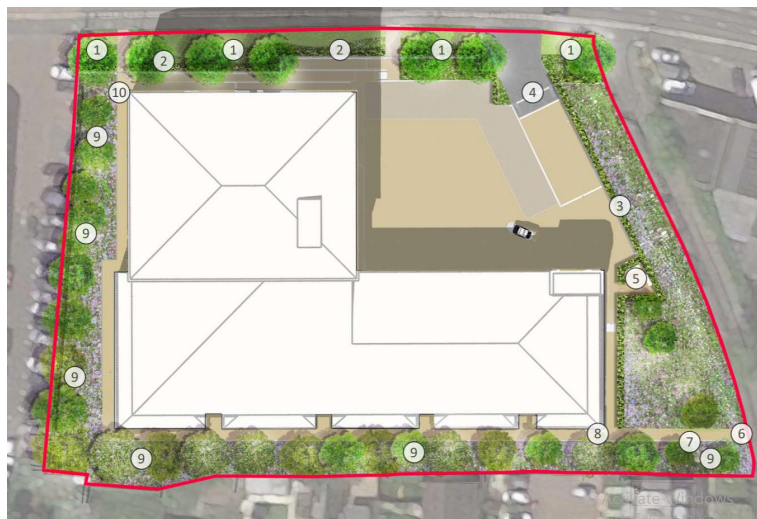


Figure 11 – Planting scheme around the site

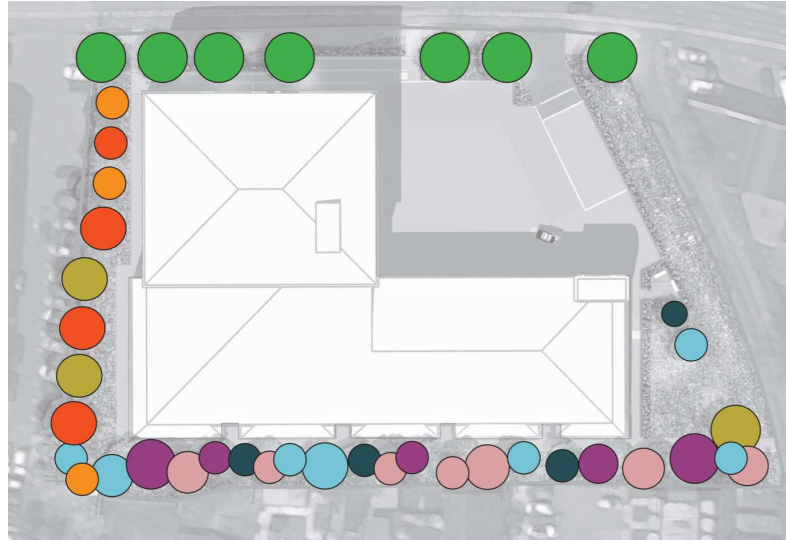


Figure 12 – Tree planting diagram

- 9.21 The landscaping scheme is a welcome addition to the proposals and will soften the proposed building from all elevations, as well as assisting in creating a better environment on Bath Road as part of the wider regeneration of the area, being a significant improvement on the barren hardstanding which currently occupies the entire site. The landscaping along Brook Path will also be a vast improvement, removing the existing fencing and introducing a grass verge improving the pedestrian environment.
- 9.22 The hard landscaping including hardstandings proposed can be seen in the paving strategy in figure 13 with the yellow areas being buff coloured block paving covering the majority of the hardstandings within the site, orange areas being charcoal coloured block paving for the parking spaces, green area being standard black top leading to access and areas of composite decking in the other area. The hard landscaping is considered necessary for the proposed development, with the majority of the hard landscaped areas allowing access and egress from the site in vehicles. The materials proposed are considered to be of high quality and with the integration of the soft landscaping is considered to provide a visual enhancement from the existing site.

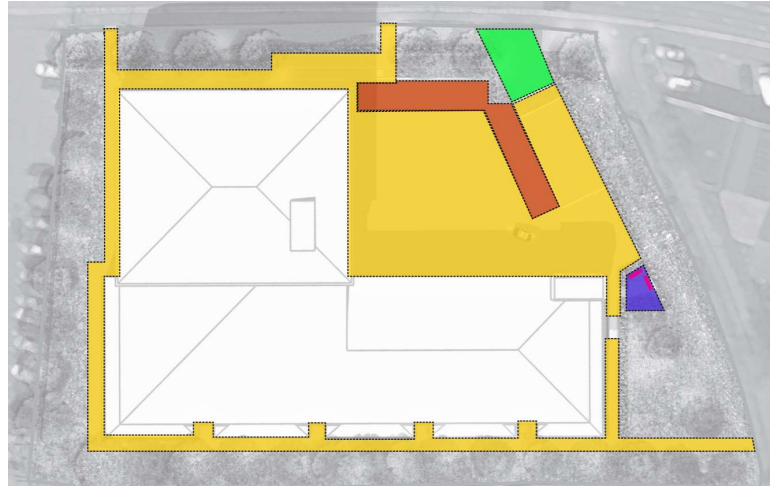


Figure 13 - proposed paving scheme

Conclusion on Design and impact on the character and appearance of the area

9.23 Officers have considered the proposed siting, layout and scale of the building on Bath Road and the residential character to the rear, south of the site. It is acknowledged that the building is of a scale and mass which is at a contrast to the existing context on the Bath Road, however; as noted above the site is located in an Area for Major Change and the comprehensive regeneration provides an opportunity for different scale buildings which contrast with the existing character. With this in mind it is considered the scale, siting and layout of the proposals are acceptable in terms of the visual relationships and impact on the surrounding area. The design of the development is considered to be to a high standard and would be compatible with future residential or mixed use development in the area, being a compatible design and using quality materials. Due regard has also been had to the quality of the existing site which is a bleak barren area of hardstanding, with the proposals considered to enhance the sites appearance. The proposed landscaping scheme will also soften the proposed building from all elevations, as well as assisting in creating a better environment on Bath Road as part of the wider regeneration of the area, being a vast improvement on the existing site. The proposals therefore are considered to have an acceptable impact on the character and visual amenity of the area and comping with Policies EN1, EN3, EMP2 and EMP12 of the Local Plan for Slough March 2004, Core Policy 8 of The Slough Local Development Framework Core Strategy 2006-2026 Development Plan Document, the objectives of SKL1 of the Site Allocations DPD and the requirements of the National Planning Policy Framework 2021.

10.0 Impact on neighbouring properties

10.1 The National Planning Policy Framework encourages new developments to be

of a high quality design that should provide a high quality of amenity for all existing and future occupiers of land and buildings. This is reflected in Core Policy 8 of the Core Strategy and Local Plan Polies EN1 and EMP2.

- 10.2 There are residential properties close to the application site. To the east/ south east are the properties along Brook Path including 1, 3, 5, 7 and 9. Number 1 Brook Path which is the closest to the site of these properties is located 15m away from the site boundary, being 24m away from the proposed building itself. To the south/ south east of the site are to properties of 1 – 21 Masons Court, with numbers 18, 19, 20, 21 and 22 being closest to the site. To the south /south west are the properties 7 – 23 Mason Road, with a new property also built at the land adj to 15 Masons Court, 15a. The properties 7 – 15 all share their rear boundary with the site, with 17 – 23 being located away from the boundary. Next to 7 Masons Court is a part of 367 Bath Road, with a section of 367 extending back to Bath Road. To the east of the site is 367 Bath Road which is a Seat/Skoda car dealership. To the north of the site to the opposite side of Bath Road is a currently vacant site, with properties along Iona Crescent beyond this and a Thrifty Car Rental.

Daylight, Sunlight, Outlook and Overbearing impact

Brook Path

- 10.3 As part of the submission a daylight and sunlight assessment has been submitted, this has been evaluated by the British Research Establishment Ltd (BRE) – Daylight and Sunlight Consultants. As part of the assessment the impacts of light on 1-3 Brook Path have been evaluated, it was considered that the impact on sunlight and daylight on these properties would be well within the BRE guidelines and it would be negligible. It is also considered given that these properties would be over 24m from lower part of the building that that loss of outlook would be acceptable and would not appear overbearing.

Mason Court and Mason Road

- 10.4 The building would have the most impact on the residential properties along Mason Court and Mason Road, being in close proximity to the boundary of the site. It is noted that the development is located to the north of all of these properties. The layout and distances from these properties can be seen in figure 14 below.



Figure 14 – Distances from residential properties to the south

10.5

To the south eastern corner of the site is 20 -22 Mason Court which has dormer windows to its front side and rear, including facing onto the site in a northerly direction at the roof level. It is noted in planning consent P/00545/019 a condition is imposed on these dormer windows as so the dormers shall only be glazed with obscure glass and opening at a level of 1.8m above internal floor level. Notwithstanding the above condition, the dormer window to the north will overlook a landscaped area of the site next to Brook Path and therefore it is not considered that outlook would be affected to this window. The outlook of windows to the front will be affected to some, with the development being within 45 degrees of these windows, however at a 45 degree angle the development is 16m away, with the landscaping area in between. Also it is noted that the current outlook is over a parking lot and therefore collectively the impact is considered acceptable. The daylight and sunlight assessment and the evaluation by BRE notes that impacts of loss of daylight and sunlight would meet guidance and be negligible. It is not considered the proposal would appear overbearing to these properties.

10.6

The dwellings 18 -19 Masons Court would be located closest to the proposed building, with the side wall of 19 Masons Court being 9m away from the rear of the building, and whilst there are no side windows facing towards the site the building will be close to the front and rear windows. The daylight and sunlight review and BRE assessment on both 18 and 19 Masons Court notes the impact on light to be minor and within guidance. The outlook to the north from both properties would be impacted with 19 being more so. At a 45 degree angle from the front and rear the building is approximately 14m away with a trees and landscaping buffer zone in between. The building would also have an impact on the rear garden area, being within 8m, with the relationship between the 2 seen in figure 15. Whilst the proposals will have an impact on outlook to some degree and will be relatively close to the residential area, it is not considered that the impact would be unacceptable, with this being the lower

part of the building, with the landscaping screen in between and separation distances. Also, as with the evaluation of 20-22 Masons Court, it is noted that the current outlook is over a parking lot. Overall it is considered that the impact would be acceptable.



10.7

Figure 15 - The rear of the proposed building on the left and 18 and 19 Mount Court on the right

10.8

The properties along Masons Road, 7-23, would be over 35m away from the proposed building, given the this separation distance a 25 degrees test was undertaken in line with Para 2.2.5 of the BRE guidance which states that states that once you have plotted the 25 degrees line *'if this is less than 25 degrees for the whole of the development then it is unlikely to have a substantial effect on the diffuse skylight enjoyed by the existing building'*. The development meets this 25 degree test and therefore it is considered impact on light is acceptable. Whilst there are outbuildings to the rear of these properties these are ancillary space not part of the main dwelling and impacts on these would be acceptable.

10.9

The visual impact of the development on Masons Road can be seen in the visuals in figure 5 and 6 above, with the full extent of the building visible. Whilst the development will be a minimum 8m from the rear boundaries of Masons Road, the inclusion of the landscaping scheme softens this impact and the distance between the main dwellings is over 35m. The 6 storey element of the building would be a further 25m into the site and therefore, whilst it is acknowledged that the building will be a vast change from the existing situation, the development would not be overbearing or result in an unacceptable level of outlook.

10.10

To the west the building is Seat/Skoda dealership, given this is not a residential building, however an assessment on the future development potential of this building is assessed below.

10.11

The development is far enough away from other dwellings to the north to ensure there is no detrimental impact.

10.12

Noise

A noise impact assessment has been submitted with the application which has been assessed by the councils Environmental Quality team. The noise assessment indicates that the noise impact of the development at the nearest sensitive receptors will be low during both the day and night, with the yard being located at the front of the site, with the proposed building shielding the existing residential receptors, mitigating noise impact on these neighbours to a degree.

10.13

The evaluation of the report notes that typical operational hours are from 08:00-18:00, however access is permitted between 05:00-23:00. Vehicles accessing the site typically includes cars (47% packing, 20% emptying), courier vans (27%) and transit users (7%). Noise data of activity at other Big Yellow sites have been reported and collated with trip data provided by i-Transport, to calculate likely noise levels at 10m from the noise source during both the day and night. This has been calculated during the day as the busiest hour with 15 vehicles using the site, based on the percentage split provided above, and during the night based on 3 light goods vehicles accessing the site during a 15 minute period. This considers a range of noise sources including vehicle movements; movements of trolleys over rough ground; fork lift trucks (where used); door and boots slams; and speech.

10.14

The orientation of the proposed development provides noise screening to receptors in the south/south east and existing buildings screen receptors to the east. This results in noise levels highest at receptors to the north of the site which do not benefit from screening, at 36dB LAeq1h and 42dB LAeq 15min during the day and night, respectively. This is equal to or lower than the background noise levels therefore noise impact is expected to be low.

10.15

Plant noise sources are expected to include small scale heating/cooling systems and extract fans. Plant details have not been specified therefore a planning condition will be imposed which limits future plant noise levels is included. Noise caused by an increase in traffic on the road network has been considered and is expected to be <1dB as traffic increase will not exceed 2%.

10.16

10.17

Construction details have not been provided therefore details of the noise from the construction phase are to be confirmed.

Overall it is considered that the impact from noise of the development on neighbouring residential uses would be acceptable, subject to safeguarding conditions.

10.18

Privacy Impacts

- There are no windows proposed to the rear with glazing being limited to the front elevation and the front sections of the east and west elevations, it is not considered therefore that there would be any loss of privacy to residential dwellings to the east, south or west. The proposed front elevation will have curtain walling exposing the inner floors, with the 1st, 2nd, 3rd, 4th and 5 floors with views to the front of the site. To the north of the site is a vacant plot but further are the properties to the southern side of Iona Crescent. These properties are located 80m from the proposed building and sit to the north of the vacant Bath Road sites. It is not considered there would be any material impact arising from the proposed development.
- 10.19

Lighting

- As part of the proposals a lighting scheme has been submitted detailing the lighting around the site, this notes that lighting will only activated between the hours of 05:00 and 23:00 except for safety and severity lighting. The lighting assessment has confirmed that in the 'worst-case scenario' when all external lights are on, and without taking into account the effect of screening by trees or planting, the levels of illuminance will not exceed 5 Lux at the site boundary, complying with relevant standards. This level of illumination will be at the front of the site only, facing on to Bath Road. As such, it is not considered that there would be adverse impacts on the amenity of the adjoining residential neighbours as a result of the proposed lighting strategy, which is consistent with Part 3a) of Core Strategy Policy.
- 10.20

Solar Panels

On the rear roof slope of the building 300m² of solar panels are proposed, these can be seen on roof in figure 1. Whilst they would cover a large area of the rear roof, it is not considered that they would be harmful to neighbouring amenity subject to a condition requiring further details of the solar panels to be agreed via condition prior to installation, ensuring they are raised from the roof by an acceptable degree and do not produce an unacceptable level of sunlight reflection, glare or glint.

Impacts on future developments

- 10.21 Impacts on future developments in the area need to be considered, including noise impacts on any future residential development, impacts on light and overlooking.
- 10.22 The layout of the building as noted in the noise section above, would reduce any impacts to the site to the west although any intensive B8 use may have impacts on the wider area through comings and goings and larger vehicles, especially to the northern site on Bath Road. As to restrict HGV's a condition visiting the site in unsociable hours, an hours restriction will be imposed on the

unit to stop HGV's entering and existing the site outside of specified hours. A condition requiring the building only to be used by Big Yellow going forward will be recommended to restrict any future intensification of the site by another user and would allow the Council the opportunity to assess such a proposal if one were to come forward on its merits..

10.23 An hours of use condition is also to be on the site to ensure the site does not operate after 23:00 or before 05:00 hours. The proposed use's core hours would be 08:00 – 18:00 with customers being able to pay a premium to access the site between 05:00-8:00 and between 18:00-23:00. Given the majority of the noise associated with the use being vehicle movements, movement of goods, doors, voices etc, with all of the storage space being inside the building, it is considered with the above conditions in place that the use would not be harmful to future residents in the area.

10.24 With regards to the loss of light to surrounding sites, both the submitted light survey (GIA Chartered Surveyors) and the BRE's light assessment (conducted as part of SBC's consultation) have modelled future development on the site. The GIA Chartered Surveyors submitted light survey used a 4 storey block on the site an equal distance from the boundary as the proposed development then undertook a façade study assessment. The study concluded that the development subject of the proposal would not unduly impact future development on the neighbouring site and would be within the BRE guidance tolerances

10.25 The BRE's assessment (conducted as part of SBC's consultation) of the site considered a notional 5 storey building on the site at an equal distance from the boundary as the subject proposals. It is acknowledged by BRE that the built form of a future neighbouring building is unlikely to be identical to that modelled, but the main residential facades of any neighbouring building would be expected to retain a reasonable distance from the site boundary. Following the modelling and calculations it was concluded that '*future residential development on the site at 367 Bath Road would retain the potential to be adequately daylit and sunlit with the proposed development at number 361 in place*'. From the studies carried out it was therefore concluded that the proposals would not harm light to the neighbouring plot to a degree which would stifle its future development.

10.26 There is a section of curtain walling proposed on the western elevation of the building which will be proposed to be obscure glazed to ensure it does not impact any future uses to the west.

10.27 Based on the above, and subject to conditions, the proposal is considered to comply with the relevant requirements of Core Policy 8 of The Core Strategy, Policies EN1 and EMP2 of The Local Plan for Slough, the objectives of SKL1 of the Site Allocations DPD and the requirements of the National Planning Policy Framework.

11.0 **Traffic and Highways**

11.1 The National Planning Policy Framework requires development to give priority first to pedestrian and cycle movements, and second - so far as possible – to facilitating access to high quality public transport. Development should be designed to create safe and suitable access and layouts which minimise conflicts between traffic and pedestrians. Plans should also address the needs of people with disabilities, allow for the efficient delivery of goods, and provide facilities for electric vehicle charging. This is reflected in Core Policy 7 and Local Plan Policies T2, T8 and EMP2. The National Planning Policy Framework states that “Development should only be prevented or refused on highways grounds if there would be an unacceptable impact on highway safety, or the residual cumulative impacts on the road network would be severe”. The objectives of SK1 of the Site Allocations DPD include, improvements to the appearance of this important main road frontage and to minimise the number of access points onto the A4.

11.2 As part of the submission a Transport Assessment has been submitted which has been evaluated by SBC’s Highways Department. The assessment of the proposed highways works and impacts are as follows.

Access

11.3 As part of the proposals it is proposed to rationalise the existing three accesses into one main access, this would, be in accordance with the objectives of SK1 of the site allocation DPD. The existing bellmouth for Brook Path and the existing use are poorly separated at present and will be separated into two separate bellmouth junctions. The applicant will be required to enter into a Section 278 agreement for the detailed design. As part of the scheme dropped kerbs and tactile paving for the crossings of the site access and Brook Path.

11.4 A 2 metre wide landscaping strip along the Brook Path along the site’s eastern boundary is proposed with a lighting column, this is to improve the highway and improve the visual appearance of the site, as well as allowing SBC to widen and further upgrade the Brook Path in the future. A legal agreement has been requested from highways to dedicate this land to the SBC in the future. However this is not considered consistent with national policy and guidance in respect of S106 agreements to be entered into with an applicant as it is not a requirement arising from the scheme to make it acceptable. The land is shown for soft landscaping and the proposed conditions would secure its use as such.

11.5 Visibility spays have been shown on a drawing confirming what can be secured at the proposed site access, with a splay of 120m to the left and 45m to the right. Visibility splays of 2.4m x 120m are required for a 40mph speed limit by

the Design Manual for Roads and Bridges (DMRB), therefore the existing right-hand visibility splay is sub-standard. SBC highways have accepted the existing visibility splays shown on the basis that the proposed development is expected to reduce the number of vehicles arriving/departing the site compared to the site's previous use as a car hire centre. It is proposed that the service yard will be raised and accessed by a ramp which is required due to the flood zone designation, this has not raised any highways concerns by SBC.

11.6 The transport statement has provided collision data was provided for the A4 Bath Road and its junctions with Elmshott Lane, Station Road, the Retail Park Signal Junction, Stowe Road and Burnham Lane. A total of 10 collisions resulting in injury were recorded within the study area, comprising nine slight and one serious injury accidents. No fatal accidents were recorded. No accidents were recorded at the site access junction in the most recent 5-year period. It is considered that the proposed development is unlikely to worsen the existing collision rate given the proposed use is forecast to generate fewer vehicle trips than the previous land use.

11.7 SBC Highways have not objected to the planning application based upon the collision recorded in the area.

Access by Sustainable Travel Modes

11.8 The site benefits from reasonable opportunities for employees to travel to work by sustainable travel modes. The site benefits from being located 220m from bus stops on the A4 Bath Road, which functions as a key bus route within Slough. There are further stops on Bower Way, which is located 420m from the site. The No. 4 service provides 2 buses per hour between Maidenhead, Slough and Heathrow between Monday and Friday. The number 5 service provides 2 buses per hour between Slough and Cippenham between Monday and Friday.

11.9 Burnham Station is located approximately 780m from the site and offers 4 trains per hour to London Paddington and Reading during the peak hours.

11.10 It is therefore considered that the site would be in a moderately sustainable location.

Trip Generation

11.11 The net change in the site's trip generation has been forecast by SBC's highways by subtracting the existing trip generation from the forecast trip generation. The existing trip generation of the site was estimated by using data from the Trip Rate Information Computer System (TRICS), the national trip generation database. For existing Trip Generation, the transport statement estimated that the existing vehicle hire company generates around 24 two-way vehicle trips in the morning peak hour and 35 two-way vehicle trips in the

11.12 evening peak hour. Across a 12-hour day, it is estimated to have generated some 400 two-way vehicle trips.

For the proposed use, the transport statement forecast Trip Generation using data collected from an existing Big Yellow store in New Cross, London. The trip rates per square metre derived from this survey site have been used to forecast 4 two-way trips during the AM Peak Hour and 7 two-way trips during the PM Peak Hour, for a facility comprising 12,580sqm. This is equivalent to one trip every 9 to 15 minutes. The transport statement forecasts that the development will generate 20 fewer two-way vehicle trips during the AM Peak Hour and 28 fewer two-way vehicle trips in the PM Peak Hour. Over a full day, the development is forecast to generate 209 fewer two-way vehicle trips. Furthermore the peak hours of operation for self-storage facilities do not coincide with traditional highway network peak hours and the peak for the Big Yellow Store at New Cross was between 1400 – 1500.

11.13 SBC Highways have raised no objection to the proposal with regards to the impact on highway capacity.

11.14 Parking

14 Car Parking Spaces are proposed, including one disabled space and one parent and child space. The size of 5 of the parking spaces to dimensions of 2.7 metres x 5.2 metres (with an additional 1.2m hatched area) has been increased during the application process by the request of SBC to enable suitable parking for vans and larger vehicles.

11.15 Swept path analysis has been provided demonstrating the three loading bays can accommodate 10m long vans or a 16.5m long articulated lorry in the unlikely event this is required.

11.16 The transport statement has provided an estimate of parking demand based upon survey data of a big yellow storage facility measuring 8,623sqm in New Cross and states that at the New Cross store there was a maximum parking accumulation of 11 vehicles during any 15-minute period across the whole survey day. SBC Highways department has considered the 14 proposed parking spaces as acceptable.

11.17 Cycle Parking

Four Sheffield stands providing eight parking spaces are proposed to serve the site. This would fall below the number required by the SBC Parking Standards; however the nature of the storage facilities as a use does not attract many cycle trips given the need to transport items to/from the storage units. The cycle parking provided is likely to be used solely by staff of the proposed unit. The number of cycle spaces is considered acceptable.

11.18 Servicing and Refuse Collection

The transport statement notes HGV trips will be infrequent; although the turning yard has been designed to allow sufficient turning space for a 16.5m articulated vehicle for instances when an articulated vehicle needs to access the site. Swept paths have been provided which demonstrates that an Articulated Vehicle measuring 16.48m in length can ingress and egress the site and it has also been demonstrated that a Phoenix 2 Duo Recycler Refuse vehicle measuring 11.2m in length can ingress and egress the site to complete refuse collection. It is also noted that in the management statement that refuse facilities will not be provided to customers, and they will be expected to take their own rubbish, such as boxes etc with them when they leave the site. It is therefore considered that the proposals are acceptable with regards to the servicing and refuse

11.19

Electric Vehicle charging points

As part of the scheme 2 electric charging points are proposed, this has been confirmed as acceptable by the borough's highways and environmental quality team and would assist in providing infrastructure for more sustainable methods of transport.

11.20

Summary and Conclusions

Based on the above, and subject to the conditions, it is considered that the proposals would not lead to severe harm to highways users. The scheme would also reduce the number of access points onto Bath Road and would improve the pedestrian environment on both Bath Road and Brook Path. The development is therefore considered to be in accordance with the requirements of Policies EMPT2, T2 and T8 of the adopted Local Plan, as well as the provisions of the NPPF.

12.0 **Flood Risk and Drainage**

12.1 Paragraph 159 of the NPPF states that inappropriate development in areas at risk of flooding should be avoided by directing development away from areas at highest risk (whether existing or future). Where development is necessary in such areas, the development should be made safe for its lifetime without increasing flood risk elsewhere.

12.2 Paragraph 161 of the National Planning Policy Framework requires all plans to apply a sequential, risk-based approach to the location of development – taking into account all sources of flood risk. Paragraph 033 of the NPPG clarifies that any development proposal should take into account the likelihood of flooding from other sources, as well as from rivers and the sea. The sequential approach to locating development in areas at lower flood risk should be applied

to all sources of flooding.

- 12.3 Core policy 8 of the Slough Core Strategy states that development will only be permitted where it is safe and it can be demonstrated that there is minimal risk of flooding to the property and it will not impede the flow of floodwaters, increase the risk of flooding elsewhere or reduce the capacity of a floodplain; and development must manage surface water arising from the site in a sustainable manner which will also reduce the risk of flooding and improve water quality
- 12.4 The site is located within flood zone 3a, defined as an area with high probability of flooding, noted in the PPG as '*Land having a 1 in 100 or greater annual probability of river flooding; or Land having a 1 in 200 or greater annual probability of sea flooding.*'. It is therefore important to assess the flood issues associated with the development.
- 12.5 As the site is located within a flood zone 3a and the development (storage and distribution) is defined as a less vulnerable development, it is required to conduct a sequential test approach, which seeks to direct development away from high flood risk areas, such as the subject site, to areas of lower flood risk. The sequential test conducted is reviewed in the Sequential text section above.
- 12.6 As part of the submission a Flood Risk Assessment, Surface Water and Foul Water Statement and Drainage and Maintenance Strategy has been submitted. The building has also been designed to take into account being located within an area of high risk to flooding, having a raised floor to allow flood water to move underneath the building. Under the building it is proposed that the building will be on stilts with the lower ground floor level set at 26.125m above ordnance datum (AOD) which is more than 0.6m above the highest Flood Zone 3, level of 25.44m AOD and flood free access will be provided from Bath Road which will not impede the flow of floodwaters.
- 12.7 The Environment Agency (EA) and the Hampshire County Council flooding risk team, who act as Slough's lead local flood authority (LLFA), have reviewed the scheme. There have been no objections raised by Hampshire Flooding to the proposals however the Environment Agency have raised objections to the scheme, however through further information submitted and amendments to the scheme these have been addressed to the satisfaction of the EA. Specifically in relation to the fencing and means of enclosure with concerns over flood water movements, it has been clarified that these will not restrict the movements of flood water due to their design, with the weld mesh fence and the pier and railings fencing permeable to water permitting the flow of any flood waters in and around the site. The EA also had concerns regarding the voids underneath the building, where it was still considered that were inadequate and that these needed to be set higher to take into account the predicted flood extent level plus climate change and that there needed to be amendments to the beam. In response to the EAs letter the beam has been redesigned so the

castellation is removed and is therefore clear from obstruction. The lower edge of the beam has also been raised to a sufficient level, without materially affecting the height of the building submitted. The EA are therefore satisfied that the proposed development meets the National Planning Policy Framework's requirements in relation to flood risk, subject to the imposition of a condition regarding flood measures which is has been included.

12.8 Measures to manage surface water and foul water are considered sufficient. The site currently being covered by a hardstanding and the proposals introduce areas of soft landscaping reducing the impermeable areas. The surface water will discharge into the public sewer along the eastern boundary and will be restricted to 2l/s for all storm events with this considered a betterment than the existing situation. With the low waste water flows generated by the self-storage use, it is considered that there will be no adverse effect on the existing foul water regime.

12.9 Overall it is considered that with the imposition of conditions that based on the above, the proposal would not increase flood risk on or elsewhere outside of the application site, and the flood risk and drainage issues would comply with Core Policy 8 of the Core Strategy, and the requirements of the National Planning Policy Framework.

13.0 **Economic Impact**

13.1 The site is located within a defined Business Area (Bath Road) as identified on the Proposals Map (2010). Core Strategy policy 5 (employment) requires there to be no loss of existing business areas to non-employment uses. Local Plan Policy EMP12 seeks a range of business developments within this Business Area to encourage its regeneration to replace any businesses that are lost. Paragraph 81 of the National Planning Policy Framework seeks planning decisions to help create the conditions in which businesses can invest, expand and adapt. Paragraph 83 of the National Planning Policy Framework seeks planning decisions to recognise and address the specific locational requirements of different sectors.

13.2 As part of the submission an Economic and Regeneration Statement has been submitted which identifies employment opportunities as part of the scheme. It is noted that there will be a number of jobs generated during the construction process, which being a sizable structure will employ a large number of workers from the local labour pool. For the size of the building the number of full time employees will be very low being between 3-4 full time staff with the addition of cleaning and maintenance jobs. This is not a positive aspect of the scheme although it is noted that for the site area the existing use of car rentals would have a similar level of employment.

13.3 Whilst noted above the site would not directly employ a large number of staff,

the business does support a significant number of indirect employment with the supporting economic statement confirming the average Big Yellow site is home to 105 businesses and other organisations with the 74 Big Yellow sites surveyed being home to 7,700 businesses. The storage areas are rented by generally small businesses, many of which are start ups. The spaces are often used to store their stock, to package stock for distribution or to store their business equipment and materials with the use of the storage areas enabling businesses to take on extra staff, or to grow their businesses.

13.4 A Quod Employment Survey has been submitted as part of the Economic Statement which notes that Big Yellow anticipates that a self-storage facility of this size, excluding the demountable mezzanines, would support approximately 113 jobs; this figure rises to approximately 449 jobs when the demountable mezzanine floors are installed. The Quod surveyed existing Big Yellow customers at other stores to estimate the number of businesses which would utilise the site. Therefore whilst the direct employment opportunities for this site are very low, it is considered the use provides great support to local businesses which in turn would assist in the creation and economic growth.

13.5 No comments have been provided by the Council's Economic Development Team on the submitted Economic Benefits Assessment. Furthermore, with the use of conditions mentioned in this report it is considered that the use would be compatible with future regeneration of the area, providing a high quality building and visual improving being a potential catalyst for further investment in the area

13.6 Based on the above, the proposal would comply with Core Policy 5 of the Core Strategy, Local Plan Policy EMP12, and the requirements of the National Planning Policy Framework.

14.0

Impact on biodiversity and ecology

14.1

Paragraph 174 of the National Planning Policy Framework requires new development to minimise impacts on biodiversity and provide net gains in biodiversity. Core Policy 9 relates to the natural environment and requires new development to preserve and enhance natural habitats and the biodiversity of the Borough.

14.2

The existing site is a vast area of hardstanding with little in plants and vegetation on the site with no areas for habitat. As part of the proposals a biodiversity net gain assessment as well has been submitted which notes that the existing site the only plants on the site are brambles, ivy, Dandelions, common sorrel, horseweed and moss. A Tree Survey Report and Arboricultural Impact Assessment has also been submitted with the application.

14.3

The proposed scheme includes vast ecology enhancements, with 0.8 ha of the site to be redeveloped, it is anticipated that circa 0.55 ha will comprise

developed land; unsealed surface (i.e. self-storage centre and associated hard landscaping), with the remainder 0.25 ha of the site will be set aside for biodiversity, comprising hedgerow planting, scattered trees, amenity grassland, shrubs and a wildflower meadow. These areas are noted in the design landscaping section of the report above, with figure 11 noting these areas. As well as the introduction of extensive planting around the site, further enhancements are proposed on the site in the form of log piles using material sourced from felled trees on site where possible; bug hotels using materials sourced in-situ where possible; bat boxes, (to be fixed away from direct illumination) above 4m on a south or west aspect, bird boxes and swift nest boxes would be fixed above 5m and on a north or east aspect.

14.4 The biodiversity report compiled has used the DEFRA's bio diversity net gain assessment tool to assess the existing and proposed ecology environment. The post development score onsite is calculated to be 1.56 biodiversity units with the existing set at 0.02 units providing a gain of +1.54 units. Given the predevelopment and post-development score the biodiversity net gain is calculated at +7715%, proving a vast improvement.

14.5 The borough's landscape and arboricultural officer has reviewed the proposals and has confirmed that the submitted Tree Survey Report and Arboricultural Impact Assessment information is sufficient to ensure established trees growing around the development would not be harmed during the construction phases and that the scheme will provide a significant uplift in biodiversity on the site. They have also recommended conditions will be imposed in relation to implementation and maintenance to ensure that planting and landscaping scheme is retained for the lifetime of the development.

14.6 Overall it is considered that the scheme would be a significant gain in biodiversity and subject to conditions is a welcome addition to the site.

15.0 **Air Quality**

15.1 Core Policy 8 of the Core Strategy seeks development to be located away from areas affected by air pollution unless the development incorporates appropriate mitigation measures to limit the adverse effects on occupiers and other appropriate receptors. Proposal should not result in unacceptable levels of air pollution. This is reflected in Paragraph 181 of the National Planning Policy Framework which also goes on to require any new development in Air Quality Management Areas and Clean Air Zones is consistent with the local air quality action plan.

15.2 The Council has recently adopted a Low Emission Strategy (LES) on a corporate basis, which is a local air quality action plan incorporating initiatives to be delivered by the Council and will set the context for revising the Local Development Plan Policies. Measures in the Low Emission Strategy include

reducing traffic and requiring electric charging points within new developments. The Low Emission Strategy (LES) is a material planning consideration but it does not form part of the current local development plan. The site is not located within a designated Air Quality Management Area.

- 15.3 An air quality assessment, has been submitted with this application. Which has been assessed by the council's environmental officer. The proposed development includes 14 parking spaces and 3 loading bays. The transport statement indicates that the proposed development will result in 191 annual average daily traffic (AADT), and 1 HGV AADT. This is under the threshold specified within the LES, Environmental Protection UK and Institute of air quality management, therefore the proposal is not expected to cause an operational air quality issue. It is also noted that this does not consider the loss of vehicle movements (previous use as vehicle hire depot), therefore the operation impacts are considered very low. Furthermore, the scheme provides 2 EV charging points this meets the standard set in the LES and therefore this is considered acceptable
- 15.4 Electric vehicle re-charging infrastructure should be provided in line with table 7 of the LES Technical Report. As the scheme includes 14 parking spaces, 2 must have access to electric charging infrastructure to meet the 10% requirement. As two fast chargers are proposed as part of the proposal, this requirement has been met.
- 15.5 Air quality impacts during construction have also been considered. The assessment submitted determines that the proposal will cause a medium dust impact during the construction phase, however this can be mitigated through the implementation of the Construction Environmental management Plan (CEMP) The borough's environmental officer has reviewed the CEMP and have considered that amendments need to be made to the CEMP and a dust management plan also needs to be submitted and therefore this will be included as a condition. A condition requiring all heating systems to meet the emission standards laid out in Table 7 of the LES Technical Report shall also be included
- 15.6 Based on the above, the proposal would comply with Core Policy 8 of the Core Strategy and the requirements of the National Planning Policy Framework 2021.

16.0 **Crime Prevention and anti-social behaviour**

- 16.1 Policy EN5 of the adopted Local Plan and Core Policy 12 require all development schemes to be designed so as to reduce the potential for criminal activity and anti-social behaviour. Paragraph 92 National Planning Policy Framework seeks development to be inclusive and safe places which are safe

and accessible, so that crime and disorder, and the fear of crime, do not undermine the quality of life or community cohesion.

As part of the security measure for the proposals a 2.4m a black weld mesh fence is proposed to the eastern, western and southern boundaries, which will provide a high level of security whilst allowing views into and out of the site, therefore assisting with natural surveillance of Brook Path. To the front of the building brick piers and railing will be used, with matching gates. The pedestrian and vehicle access gate will be open during the hours that staff are on Site (08:00-18:00 core hours), whilst during the extended hours, the gates will be shut; customers with extended access hours will have a pin to operate the security gate, enabling them to open the gates to access the yard area and to access the building itself. Outside of the extended hours, no access will be permitted to the site and this is to be controlled through the security access system. A lighting impact assessment has been submitted with the application which notes that security lighting will be in operation at the building will assist in site security at night.

16.2 It has been noted in the Design and Access statement that the site will aspire to meet a Security Assurance by the Building Research Establishment (SABRE) rating of EXCELLENT being a high level of security at the site.

16.3 As noted in the CEMP during construction a secure boundary fence (timber framed painted plywood hoarding) will be erected for the duration of the proposed construction activities to protect members of the public from the dangers of the ongoing construction works. The hoarding is to be located along the current boundary line to ensure the construction site is sufficiently secure to deter and prevent entry to the site by unauthorised persons and to prevent removal of materials or goods from the site. A dedicated operative would be allocated to the access points to ensure that unauthorised entry is not permitted and to control traffic movement into and out of the development.

16.4 Overall it is considered that appropriate security measures are in place and the scheme would not increase crime or the fear of crime within the wider area and the proposal would accord with the in Core Policy 12 of the Core Strategy Local Plan Policy EN5, and the requirements of the National Planning Policy Framework

17.0 **Contaminated Land**

17.1 Paragraphs 183 and 184 of the National Planning Policy Framework require a site to be suitable for its proposed use taking account of ground conditions and any risks arising from land instability and contamination. This is reflected in Core Policy 8 of the Core Strategy.

17.2 As part of the submission a Geotechnical and Geoenvironmental desktop study has been submitted which has been evaluated by the council's environment

officer. The preliminary assessment has identified the site to have a medium to high sensitivity for hydrogeology, and low sensitivity for human health receptors. The overall site risk in relation to potential contamination issues has been identified as low to moderate, however other potential sources of contaminations have been identified that required further investigation and assessment in order to correctly quantify the risks associated with the development and to future site users. Both the environmental officer and submitted report recommends further investigations be carried out, this will be conditioned.

17.3

Based on the above, subject to conditions the proposal would accord with the in Core Policy 8 of the Core Strategy, and the requirements of the National Planning Policy Framework.

18.0 **Sustainable design and construction**

18.1 Core Policy 8 of the Core Strategy seeks to minimise the consumption and unnecessary use of energy; generate energy from renewable resources; and incorporate sustainable design and construction techniques. Paragraph 7.159 states proposals for non-residential development should achieve a BREEAM rating of “very good” or “excellent”.

18.2 The Developers Guide Part 2 expects commercial development of 10,000 sqm or more to achieve a BREEAM rating of excellent along with low or zero carbon energy generation equivalent to approximately 10% of the developments carbon emissions. A percentage score of over 55% is required to achieve BREEAM ‘Very Good’. A percentage score of over 70% is required to achieve BREEAM ‘Excellent’.

18.3 In the supporting sustainability appraisal it is noted that the intention is to target a BREEAM rating of ‘Excellent’. However, it is noted that design limitations of such a scheme that nearly 12% of the predicted score is dependent on the appointed contractor, site functionalities and available procurement routes at the point of construction. The proposal is therefore committed to achieving ‘Very Good’ as a minimum, whilst targeting an ‘Excellent’ rating.

18.4 The submitted plans propose a 300sqm area on the southern roof of the building for photovoltaic panels, an Air Source Heat Pump is also proposed. A 50kW peak installation distributed evenly across the building, will have a total area of approximately 300m². This will generate 37,839 kWh/yr, which equates to a carbon reduction of 19,638 kgCO₂/yr, (49%). Air source heat pumps are a viable technology for the reception and managers office. A 1% reduction in CO₂ emissions has been calculated where the heat pump is installed as part of a reverse cycle refrigeration system providing both space heating and cooling. The effect of the renewable technologies for the building as described above is

to reduce the building regulated emission by 49%, 20 Tonnes of CO2 per annum.

18.5 The total cumulative savings for the development equates to 21 Tonnes CO2 per annum a reduction in Regulated Carbon Dioxide savings of 51%.

The proposed very good BREEAM rating broadly complies with Core Policy 8 which requires such schemes to achieve a 'very good' or 'excellent'. The proposals do not fully accord with Developers Guide Part 2 which seeks a BREEAM rating of excellent for a development of this size, however it is noted that the scheme will strive for excellent and conditions are included to ensure this a minimum of very good is achieved. Based on the above, the proposal would comply with Core Policy 8 of the Core Strategy and is considered acceptable.

19.0 **Equalities Considerations**

19.1 Throughout this report, due consideration has been given to the potential impacts of development, upon individuals either residing in the development, or visiting the development, or whom are providing services in support of the development. Under the Council's statutory duty of care, the local authority has given due regard for the needs of all individuals including those with protected characteristics as defined in the 2010 Equality Act (e.g.: age (including children and young people), disability, gender reassignment, pregnancy and maternity, race, religion or belief, sex and sexual orientation. In particular, regard has been had with regards to the need to meet these three tests:

- Remove or minimise disadvantages suffered by people due to their protected characteristics;
- Take steps to meet the needs of people with certain protected characteristics; and;
- Encourage people with protected characteristics to participate in public life (et al).

19.2 It is considered that there would only be temporary (but limited) adverse impacts upon all individuals, with protected characteristics, whilst the development is under construction, by virtue of the construction works taking place. People with the following characteristics have the potential to be disadvantaged as a result of the construction works associated with the development e.g.: people with disabilities, maternity and pregnancy and younger children, older children and elderly residents/visitors. It is also considered that noise and dust from construction has the potential to cause nuisances to people sensitive to noise or dust. However, measures under other legislation covering environmental health should be exercised as and when required.

19.3 In conclusion, it is considered that the needs of individuals with protected characteristics have been fully considered by the Local Planning Authority exercising its public duty of care, in accordance with the 2010 Equality Act.

20.0 **Presumption in favour of sustainable development**

20.1 The application has been evaluated against the Development Plan and the NPPF and the Authority has assessed the application against the core planning principles of the NPPF and whether the proposals deliver “sustainable development.” This report identifies that the proposal would comply with the relevant policies in the Development Plan. The application is therefore recommended for approval subject to conditions.

21.0 **PART C: RECOMMENDATION**

21.1 Having considered the relevant policies set out above, and all other relevant material considerations it is recommended the application be delegated to the Planning Manager:

A) Delegate to the planning manager for approval subject to finalising conditions.

22.0 **PART D: LIST CONDITIONS AND INFORMATIVES**

CONDITIONS:

1. The development hereby permitted shall be commenced within three years from the date of this permission.

REASON To prevent the accumulation of planning permissions, and to enable the Council to review the suitability of the development in the light of altered circumstances and to comply with the provisions of Section 91 of the Town and Country Planning Act 1990.

2. The development hereby approved shall be implemented only in accordance with the following plans and drawings hereby approved by the Local Planning Authority:

(a) Application Form

(b) Covering Letter, Dated: 19/03/2021, Received: 22/03/2021

(c) Site Location Plan, Plan Number: 2102-X01, Dated: 05/02/2021,

Received: 22/03/2021

(d) Existing Site Plan, Plan Number: 2102-X02, Dated: 11/02/2021, Received: 22/03/2021

(e) Design and Access Statement, Dated: March 2021, Received: 22/03/2021

(f) Planning Statement, Dated: March 2021, Received: 22/03/2021

(g) Economic and Regeneration Statement, Dated: March 2021, Received: 22/03/2021

(h) Big Yellow Management Plan, Dated: March 2021, Received: 22/03/2021

(i) Flood Risk Assessment, Dated: March 2021, Received: 22/03/2021

(j) Drainage Strategy and Maintenance Statement, Dated: March 2021, Received: 22/03/2021

(k) Foul Water and Surface Water Management Statement, Received: 22/03/2021

(l) Preliminary Ecological Appraisal, Dated: March 2021, Received: 22/03/2021

(m) Biodiversity Net Gain Assessment, Dated: 8th March 2021, Received: 22/03/2021

(n) Tree Survey Report and Arboricultural Impact Assessment, Dated: 9th March 2021, Received: 22/03/2021

(o) Transport Statement Dated: 16th March 2021, Received: 22/03/2021

(p) Noise Assessment, Dated: 24th February 2021, Received: 22/03/2021

(q) Multi-Utility Infrastructure Assessment, Dated: March 2021, Received: 22/03/2021

(r) Daylight and Sunlight Report, Dated: 18th March 2021, Received: 22/03/2021

(s) Energy Assessment Revision V 1.0, Dated: 15th March 2021, Received: 22/03/2021

(t) Sustainability Appraisal Version 1.0, Dated: February 15th March 2021, Received: 22/03/2021

(u) Landscape Design Report, Dated: 15th March 2021, Received: 22/03/2021

(v) Air Quality Assessment Rev 1, Dated: 2nd March 2021, Received: 22/03/2021

(w) Lighting Impact Assessment Version v2, Dated: 15th March 2021, Received: 22/03/2021

(x) Construction and Environmental Management Plan, Dated: February 2021, Received: 22/03/2021

(y) Geotechnical and Geoenvironmental Desktop Study, Dated: March 2021, Received: 22/03/2021

(z) Amended Flood Risk Sequential Test, Dated: August 2021, Received: 31/08/2021

(aa) Proposed Ground Floor Site Plan, Plan Number: 2102-P01-K, Received: 23/09/2021

(bb) Proposed North and East Elevations, Plan Number: 2102-P07-B, Received: 15/08/2021

- (cc) Proposed South and West Elevations, Plan Number: 2102-P08-B, Received: 15/08/2021
- (dd) Proposed Section AA, Plan Number: 2102-P09-A, Received: 15/08/2021
- (ee) Proposed Roof Plan, Plan Number: 2102-P02-G, Received: 21/07/2021
- (ff) Proposed First Floor Mezzanine, Plan Number: 2102-P03-A, Received: 22/03/2021
- (gg) Proposed Second Floor Mezzanine, Plan Number: 2102-P04-A, Received: 22/03/2021
- (hh) Proposed Third and Fourth Floor Mezzanine, Plan Number: 2102-P05, Received: 22/03/2021
- (ii) Proposed Fifth Floor Mezzanine, Plan Number: 2102-P06, Received: 22/03/2021
- (jj) Amended Fence and Void Details as Proposed, Plan number: 2102-P10-B, Dated 08/02/2021, Received 10/08/2021
- (kk) Campbell Reith, Response to EA objection letter 1, Received 09/06/2021
- (ll) Campbell Reith, Response to EA objection letter 2, Received 10/08/2021

REASON To ensure that the site is developed in accordance with the submitted application and to ensure that the proposed development does not prejudice the amenity of the area and to comply with the Policies in the Development Plan.

3. The development hereby permitted shall only be managed and operated by Big Yellow Self Storage Company Limited (or any other nominated group company) and in accordance with the Big Yellow Management Plan, received 22/03/2021.

Reason: To ensure the development operates in accordance with the submitted sequential test and to safeguard the amenities of occupiers of the adjoining properties and future residential dwellings in the area in accordance with 8 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008, Policies EN1 and EMP2 of the Adopted Local Plan for Slough 2004, the Slough LDF site allocations DPD (2010) and the requirements of the National Planning Policy Framework 2021.

4. Heavy Goods Vehicles (HGVs) shall not enter and exit the site outside the hours of 08:00 - 18:00.

REASON: To ensure that the development hereby permitted is not detrimental to the amenity of the surrounding area by reason of undue noise emission and/or unacceptable disturbance. In accordance with Policy

EN1 and EMP2 of The Adopted Local Plan for Slough 2004, Core Policy 8 of the Slough Local Development Framework Core Strategy 2006-2026, and the National Planning Policy Framework.

5. The development shall not be occupied until the proposed new access has been constructed in accordance with Mountford Pigott Drawing No. 2102-P01-K, full details to be submitted to and approved in writing by the Local Planning and Highways Authority prior to occupation.

REASON: In order to minimise danger, obstruction and inconvenience to users of the highway and of the development.

6. The development hereby permitted shall not be occupied until all the visibility splays shown on the approved drawings have been provided. Those areas shall at all times thereafter be kept free of all obstructions to visibility over a height of 600mm measured from the surface of the nearside channel level of the carriageway.

REASON: To provide adequate intervisibility between the access and the existing public highway for the safety and convenience of users of the highway and of the access.

7. The development shall not be occupied until the associated vehicle parking and turning spaces have been surfaced and marked out in accordance with approved Mountford Pigott Drawing No. 2102-P01-K. The spaces shall thereafter be kept available for parking at all times.

REASON: To enable vehicles to draw off, park and turn clear of the highway to minimise danger, obstruction and inconvenience to users of the adjoining highway.

8. The 2 metre wide strip of land on Brook Path, highlighted on Mountford Pigott Drawing No. 2102-P01-K, will be retained for landscaping and provision of a lighting column.

REASON: To ensure a satisfactory appearance of the development so as not to prejudice the visual amenity of the locality and to improve the pedestrian environment along Brook Path

9. The development shall not be occupied until details of the cycle parking provision (including location, housing and cycle stand details) have been submitted to and approved in writing by the Local Planning Authority. The cycle parking shall be provided in accordance with these details prior to the occupation of the development and shall be retained at all times in the future for this purpose.

REASON: To ensure that there is adequate cycle parking available at the site in accordance with Policy T8 of The Local Plan for Slough 2004, and to meet the objectives of the Slough Integrated Transport Strategy

10. Should the findings of the Phase 1 Desk Study approved pursuant to the Phase 1 Desk Study condition identify the potential for contamination, development works shall not commence until an Intrusive Investigation Method Statement (IIMS) has been submitted to and approved in writing by the Local Planning Authority. The IIMS shall be prepared in accordance with current guidance, standards and approved Codes of Practice including, but not limited to, BS5930, BS10175, CIRIA 665 and BS8576. The IIMS shall include, as a minimum, a position statement on the available and previously completed site investigation information, a rationale for the further site investigation required, including details of locations of such investigations, details of the methodologies, sampling and monitoring proposed.

REASON: To ensure that the type, nature and extent of contamination present, and the risks to receptors are adequately characterised, and to inform any remediation strategy proposal and in accordance with Policy 8 of the Core Strategy 2008.

11. Development works shall not commence until a Quantitative Risk Assessment (QRA) has been prepared for the site, based on the findings of the intrusive investigation. The risk assessment shall be prepared in accordance with the Contaminated Land report Model Procedure (CLR11) and Contaminated Land Exposure Assessment (CLEA) framework, and other relevant current guidance. This must first be submitted to and approved in writing by the Local Planning Authority and shall as a minimum, contain, but not limited to, details of any additional site investigation undertaken with a full review and update of the preliminary Conceptual Site Model (CSM) (prepared as part of the Phase 1 Desk Study), details of the assessment criteria selected for the risk assessment, their derivation and justification for use in the assessment, the findings of the assessment and recommendations for further works. Should the risk assessment identify the need for remediation, then details of the proposed remediation strategy shall be submitted in writing to and approved by the Local Planning Authority. The Site Specific Remediation Strategy (SSRS) shall include, as a minimum, but not limited to, details of the precise location of the remediation works and/or monitoring proposed, including earth movements, licensing and regulatory liaison, health, safety and environmental controls, and any validation requirements.

REASON: To ensure that potential risks from land contamination are adequately assessed and remediation works are adequately carried out, to safeguard the environment and to ensure that the development is suitable for the proposed use and in accordance with Policy 8 of the Core Strategy

2008.

12. No development within or adjacent to any area(s) subject to remediation works carried out pursuant to the Phase 3 Quantitative Risk Assessment and Site Specific Remediation Strategy condition shall be occupied until a full Validation Report for the purposes of human health protection has been submitted to and approved in writing by the Local Planning Authority. The report shall include details of the implementation of the remedial strategy and any contingency plan works approved pursuant to the Site Specific Remediation Strategy condition above. In the event that gas and/or vapour protection measures are specified by the remedial strategy, the report shall include written confirmation from a Building Control Regulator that all such measures have been implemented.

REASON: To ensure that remediation work is adequately validated and recorded, in the interest of safeguarding public health and in accordance with Policy 8 of the Core Strategy 2008.

13. The rating level of noise emitted by all fixed plant on the site shall not exceed 5dB below background noise levels, at 1m of the sensitive receptor façade. The measurement and assessment shall be made according to BS 4142:2014+A1

REASON: To ensure that the proposed development does not prejudice the amenities of occupiers of neighbouring properties.

14. Unless otherwise agreed in writing by the Local Planning Authority, the development hereby permitted shall not be open outside of the following hours: 05:00 - 23:00 Monday to Sunday.

REASON: To ensure that the proposed development does not prejudice the amenities of occupiers of neighbouring properties.

15. Unless otherwise agreed in writing by the Local Planning Authority, the windows/ Curtain walling in the western elevation of the development hereby permitted shall be obscure glazed to the equivalent to Pilkington Level 3 or above and shall be permanently retained as such.

REASON To minimise any loss of privacy to adjoining occupiers or future occupiers.

16. No development shall take place until a Construction & Environmental Management Plan (CEMP) has been submitted to and approved in writing by the Local Planning Authority, which shall include details of the provision to be made to accommodate all site operatives', visitors' and construction vehicles loading (to a minimum Euro 6/VI Standard), off-loading, parking and turning within the site and wheel cleaning facilities during the construction period, non-road mobile machinery (NRMM) controls to be in line with Table 10 in the Low Emission Strategy (LES) guidance and that all

heating systems (when the development is operational) shall meet the emission standards laid out in table 7 of the LES guidance. The Construction & Environmental Management Plan shall include details of the controls and measures that will be environmental effects of demolition and construction work. The scheme shall include:

- (i) control of noise
- (ii) control of dust, smell and other effluvia
- (iii) control of surface water run off
- (iv) site security arrangements including hoardings
- (v) proposed method of piling for foundations

The Plan shall thereafter be implemented as approved before development begins and be maintained throughout the duration of the construction works period.

REASON: In the interest of minimising danger and inconvenience to highway users and in the interests of air quality and in the interests of the amenities of the area in accordance with Core Policies 7 and 8 of the Slough Local Development Framework Core Strategy 2006-2026, and the guidance set out in the National Planning Policy Framework (2019).

17. Prior to the first occupation of the development the 2 Electric Vehicle Charging Points shown on plan number 2102-P01-K shall be installed and retained thereafter.

REASON: To ensure vehicle parking is provided and encourage up-take of electric vehicle use, in accordance with Policy T2 of the Adopted Local Plan (2004), Policies 7 and 8 of the Core Strategy 2008, the guidance contained in the Council's Developer's Guide Part 3 (2008) and the National Planning Policy Framework 2021.

18. Prior to the first use of the development hereby approved a Design Stage Certificate shall be submitted to and approved by the Local Planning Authority confirming that the development has been designed to achieve a minimum standard of BREEAM Very Good (or equivalent standard).

REASON In the interest of sustainable development in accordance with Policy 8 of the Core Strategy 2008, and the requirements of the National Planning Policy Framework 2021.

19. Within 6 months of the development hereby approved being brought into first use a Post-Construction Review Certificate confirming the development hereby approved has been constructed so as to achieve a minimum standard of BREEAM Very Good (or equivalent standard) shall be submitted to and approved the Local Planning Authority.

REASON In the interest of sustainable development in accordance with policy 8 of the Core Strategy of the Core Strategy 2008, and the

requirements of the National Planning Policy Framework 2021.

20. No external lighting other than that shown on drawing number 200248 E 001 Rev P2 contained in the Dawson and Partners Lighting Impact Assessment shall be installed on the site. All lighting installed shall be operated in accordance with the lighting assessment.

REASON To ensure the safety of aviation and in the interests of the amenities of the area, in order to comply with Policy 8 of the Core Strategy 2008, and the requirements of the National Planning Policy Framework 2021

21. Prior to any construction works above ground floor slab taking place, samples of new external finishes and materials (including, reference to manufacturer, specification details, positioning, and colour) to be used in the construction of the external envelope of the development hereby approved shall be submitted to and approved in writing by the Local Planning Authority and the development shall be carried out in accordance with the details approved.

REASON To ensure a satisfactory appearance of the development so as not to prejudice the visual amenity of the locality in accordance with Policy EN1 of The Adopted Local Plan for Slough 2004.

22. Prior to any construction works above ground floor slab taking place, the external ground surface materials to be used in the construction of the access and circulation roadways, pathways and communal areas within the development hereby approved shall be submitted to and approved in writing by the Local Planning Authority. The development shall be carried out in accordance with the details approved prior to first occupation of the buildings.

REASON To ensure a satisfactory appearance of the development so as not to prejudice the visual amenity of the locality in accordance with Policy EN1 of The Local Adopted Plan for Slough 2004.

23. No development hereby permitted shall commence until the tree protection measures detailed in the submitted Tree survey report and arboricultural impact assessment by the rps group have been implemented, and these measures shall be provided and maintained during the period of construction works.

REASON To ensure the satisfactory protection of trees to be retained in the interest of visual amenity and to meet the objectives of Policy EN3 of The Local Plan for Slough 2004, Core Policy 8 of The Slough Local Development Framework Core Strategy 2006-2026 Development Plan Document, and the National Planning Policy Framework.

24. The external areas of the development hereby approved shall be landscaped as set out in the plans and supporting information within the Outerspace Landscape Design Report received 22/03/2021 and in no other way unless and otherwise approved in writing by the Local Planning Authority.

The approved scheme shall be carried out no later than the first planting season following completion of the development. Within a ten year period following the implementation of the scheme, if any of the new or retained trees or shrubs should die, are removed or become seriously damaged or diseased, then they shall be replaced in the next planting season with another of the same species and size as agreed in the landscaping tree planting scheme by the Local Planning Authority.

REASON: Required to safeguard and enhance the character and amenity of the area, to provide ecological, environmental and bio-diversity benefits and to maximise the quality and usability of open spaces within the development, and to enhance its setting within the immediate locality in accordance with Core Policy 8 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan Document, December 2008 and Policy EN3 of The Adopted Local Plan for Slough 2004.

25. Prior to first occupation of the development a 25 year maintenance programme following planting shall be submitted and approved by the local authority. Following planting the maintenance plan will be adhered to thereafter.

REASON To ensure the landscaping scheme is adequately maintained in the interests of the visual amenity of the area and accordance with Policy EN3 of The Adopted Local Plan for Slough 2004.

26. Prior to first occupation of the development hereby approved, details of the locations of the ecological enhancements including swift nest boxes, log piles, bug hotels, bat boxes and bird boxes shall be submitted to and approved by the LPA. The ecological enhancements shall be retained thereafter.

REASON: To provide ecological, environmental and bio-diversity benefits in accordance with Core Policy 9 of The Slough Local Development Framework, Core Strategy 2006 – 2026,.

27. Notwithstanding the provisions of the Town & Country Planning (General Permitted Development)(England) Order 2015, (or any Order or Statutory Instrument revoking and re-enacting that Order), no windows, other than

those hereby approved, shall be formed in any elevations of the development without the prior written approval of the Local Planning Authority.

REASON To ensure the visual character and appearance of the facades are preserved in accordance with Policies EN1 and of The Adopted Local Plan for Slough 2004 and to ensure the development does not prejudice the future development of adjoining lands; so, as to protect the privacy of neighbouring properties and to protect the visual amenity of existing properties in accordance with Core Policy 8 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan

28. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (as amended) (or any Order revoking or re-enacting that Order with or without modification), the land uses hereby permitted falling within B8 as defined by Town and Country Planning (Use Classes) Order 1987 (as amended) (or any Order revoking or re-enacting that Order with or without modification) shall not be used for any other use without express planning consent from the LPA.

REASON: To protect the residential amenity of surrounding properties in accordance with Core Policy 8 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan.

29. Prior to first use of the development hereby approved, details of the photovoltaic array shown on plan number 2102-P02-G received 21/07/2021 shall be submitted to and approved in writing by the LPA. The photovoltaic array shall then be installed and operated in accordance with the approved details.

REASON: To protect the residential amenity of surrounding properties in accordance with Core Policy 8 of The Slough Local Development Framework, Core Strategy 2006 – 2026, Development Plan.

30. Prior to first occupation of the development hereby permitted, the gates, walls and fencing shall be erected as shown of the approved plans and shall include flood mitigation measures shown on plan number 2102-P10-B Fence and void details as proposed.

Reason: To ensure the visual character and appearance of the area is not harmed, that the site is secure and would not restrict the follow of flood water.

31. The development shall be carried out in accordance with the submitted flood risk assessment reference 12904-CRH/ZZ/XX/RP/C/0001_FRA P2 (dated March 2021 by CambellReith) and the following mitigation it details:

- Finished floor levels shall be set no lower than 26.10 metres above

Ordinance Datum (AOD)

- Flood storage as shown in Table 8.2 and Appendix H and the letter in response to EA objection (dated August 2021, reference: FGAfga12904-20210809-EA P3)

and drawing reference 2102-P10-B - Fence and Void Details as Proposed (dated 02/08/2021)

These mitigation measures shall be fully implemented prior to occupation and subsequently in accordance with the scheme's timing/phasing arrangements. The measures detailed above shall be retained and maintained thereafter throughout the lifetime of the development.

Reasons:

- To reduce the risk of flooding to the proposed development and future occupants
- To prevent flooding elsewhere by ensuring that compensatory storage of flood water is provided

INFORMATIVE(S):

1. It is the view of the Local Planning Authority that the proposed development does improve the economic, social and environmental conditions of the area for the reasons given in this notice and it is in accordance with the National Planning Policy Framework.
2. Prior to commencing works the applicant will need to enter into a Section 278 Agreement of the Highways Act 1980 / Minor Highway Works Agreement with Slough Borough Council for the implementation of the works in accordance with the approved drawings (Mountford Pigott Drawing No. 2102-P01-K)
3. The development must be so designed and constructed to ensure that surface water from the development does not drain onto the highway or into the highway drainage system.
4. The applicant is advised that if it is intended to use soakaways as the method of dealing with the disposal of surface water then the permission of the Environment Agency will be necessary.
5. The permission hereby granted shall not be construed as authority to obstruct the public highway by the erection of scaffolding, hoarding, skip or any other device or apparatus for which a license must be sought from the Highway Authority.
6. This permission shall not be deemed to confer any right to obstruct the Public Right of Way crossing or abutting the site which shall be kept open and unobstructed until legally stopped up or diverted under section 257 of the Town and Country Planning Act 1990.
7. The reference to "or any other nominated group company" at Condition 3 is intended to include; Another Big Yellow Group company; A re-branding of

the organisation to operate under a new name; and a merge or takeover of the organisation.

8. Thames Water:

The proposed development is located within 15m of Thames Waters underground assets, as such the development could cause the assets to fail if appropriate measures are not taken. Please read our guide 'working near our assets' to ensure your workings are in line with the necessary processes you need to follow if you're considering working above or near our pipes or other structures.

<https://developers.thameswater.co.uk/Developing-a-large-site/Planning-your-development/Working-near-or-diverting-our-pipes>.

Should you require further information please contact Thames Water.

Email: developer.services@thameswater.co.uk

Thames Water will aim to provide customers with a minimum pressure of 10m head (approx 1 bar) and a flow rate of 9 litres/minute at the point where it leaves Thames Waters pipes. The developer should take account of this minimum pressure in the design of the proposed development.

Appendix 1 Sequential test data

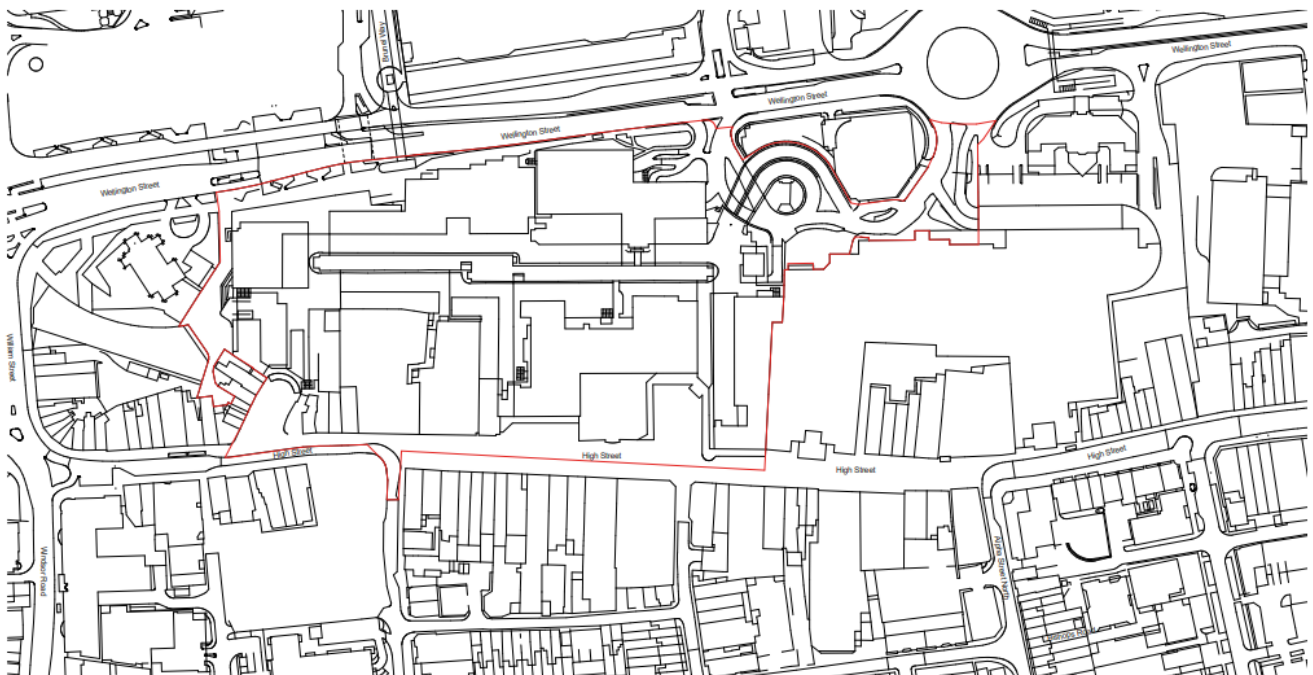
Sequential Test - August 2021													Aug-21	
	Address of site	Local Authority	Site Area (approx./ acres)	Flood Zone	Source	Vacant or Current Use	Allocated in Local Plan	Ownership	Planning Status	Development Constraints	Being pursued by others	Availability	Suitability	Comments and Conclusion
1	Logistics City Maidenhead, Cordwallis Industrial Estate, Clivemont Road, Maidenhead, SL6 7DE	RBWM	1.37	1	Property Agent Website/ Egi	Site Under Construction	Located in allocated Business Area	Private	Secured and under construction	Accessibility and Visibility	Yes - Majority Under Offer/ no longer on market. Two parcels remain on market.	Under Offer	No due to access and visibility. Being pursued by others.	Site under construction. New Warehouse Unit (B2, B8 & Class E). of 32,399 sq ft warehouse unit, with up to 64 parking spaces. Planning granted July 2019 (ref. 19/00989/FULL). The store under construction would not meet Big Yellow's requirements as a larger store with a much smaller yard and less parking would be required by Big Yellow on the plot. The site is located on a minor road, with poor visibility and very limited passing traffic. The access route to the site passes along residential roads. Big Yellow already own a small site on this road which they use to store Big Yellow's operational equipment. It is a location that is therefore well known to the business and which they know is not suitable for a trading Big Yellow Store. The site is also under offer with another party (agent's website).
2	The Link, Westacott Way, Maidenhead, Berkshire, SL6 3QH	RBWM	Building floorspace only available, not site	1	Property Agent Website/ Egi	New Build Purpose Built Office	Located in Established Employment Area in Green Belt	Private	Secured and built out	Access, visibility, Green Belt location, use, tenure	Not currently - on market	On market - to let only	No due to construction of property for B1 use and tenure.	New build purpose built Grade A office space totalling 69,209 sq ft. Site has been built out for office use and not suitable for B8 use. Situated in landscaped office park with 344 Car Parking Spaces. Site only available on leasehold basis. Site not suitable for Big Yellow. The site is also located on a non main road with poor visibility and limited passing traffic. Green Belt location.
3	The Triangle Site (land south of the A308(M) west of Ascot Road and north of the M4), Maidenhead	RBWM	63.5	2/3	RBWM Site Allocation	Vacant	Yes - Industrial and Warehouse uses	Private	None	Not on market. Flood Risk.	Not on market.	No	No - not available. Site area exceed BYSS requirement.	Mid 90s application refused for service station. No relevant applications for development since. The site allocation requires a comprehensive master plan to be developed for the site. Big Yellow would only require a very small quantum of the land. The site is not currently available and it is not known if or when planning will be pursued on the site, what the development will comprise of or if the site will be brought to the market.
4	Crossrail West Outer Depot, Maidenhead (To the south of Boyn Valley Road)	RBWM	2.9	1	RBWM Site Allocation	Crossrail Depot	Yes - Industrial and Warehouse uses	Network Rail	None	Not on market. Proximity to railway. Shape of site.	Not on market.	No	No - not available. Site shape would not be suitable for Big Yellow. Nor would location.	The site has been used as a Crossrail depot. No relevant recent planning history. It is not currently available. However, notwithstanding this it would not be suitable for a Big Yellow as a result of its access and poor visibility from the road network. It is also a very long and narrow site, which would not be suitable for Big Yellow's requirements.
5	Magnet Leisure Centre And Magnet Leisure Complex Car Park Holmanleaze Maidenhead	RBWM	4.94	Eastern part 2/3. Remainder 1	RBWM Application Search	Former Leisure Centre/ Car Park	Local Plan identifies closure of Leisure Centre	Private	Application - not yet determined	Being pursued for residential led redevelopment.	Yes - Residential Developer	No. Planning being pursued by Countryside Properties (UK) Limited	Site area exceed BYSS requirement. Being pursued for alternative use.	Planning App submitted Feb 21 (ref. 21/00502/FULL): Construction of x5 buildings to create x439 homes (use class C3) with car and cycle parking, pedestrian and vehicular access alterations, landscaping works and boundary treatments, following demolition of all existing buildings and structures. Application not determined. Site is not available as it is being pursued by a residential developer for residential uses.
6	York Road Redevelopment Area York Road Maidenhead SL6 1RF	RBWM	4	1	RBWM Application Search	Under construction	Yes for Residential, community and retail	JV scheme with RBWM and Private Developer	Planning permission granted for residential led redevelopment.	Being pursued for residential led redevelopment.	Yes - Residential Developer	No. Planning secured by Countryside Properties (UK) Limited	Development under construction	Planning granted in 2018 (ref. 18/01608/FULL) for: Mixed use redevelopment of the site comprising of 5 no. buildings 4-8 storeys in height to provide 229 new residential dwellings (Use Class C3), 1,930 sqm GEA of commercial and community/cultural floor space (Use Class A1/A3/B1/D1), provision of a new civic square and public realm enhancements, along with car parking, access, roads, landscaping and other associated works following demolition and clearance of all existing structures. Site is not available as it is being pursued by a residential developer for

	Address of site	Local Authority	Site Area (approx./ acres)	Flood Zone	Source	Vacant or Current Use	Allocated in Local Plan	Ownership	Planning Status	Development Constraints	Being pursued by others	Availability	Suitability	Comments and Conclusion
7	Moorbridge Court And Liberty House At 29 To 53 Moorbridge Road Maidenhead	RBWM	1.2	2	RBWM Application Search	Existing Offices and car parking.	No	Private	Planning secured for residential development.	Being pursued for residential led redevelopment.	Yes - Residential Developer	No. Planning secured by Bellway Homes.	Being pursued for residential.	Planning secured in 2021 (ref. 20/01129/FULL) for: Construction of 5 residential blocks comprising of 129 residential units together with associated landscaping, car parking and infrastructure works following the demolition of the existing buildings. Site is not available as it is being pursued by a residential developer for residential uses.
8	Hitachi Europe Ltd Whitebrook Park 68 Lower Cookham Road Maidenhead SL6 8YA	RBWM	6.1	2/3	RBWM Application Search	Offices.	Yes - Business Area	Private	No.	Flood Risk. Non urban location.	Site not currently available. Occupied as offices.	No. Planning being pursued for conversion of existing building or demolition.	Potentially being pursued for residential. Not available or suitable location for Big Yellow.	21/00598/DEM: Prior notification for the demolition of the existing building. Submitted by Hitachi. Approved. Also application to convert offices to residential (88 units) pending consideration. A previous application for conversion was refused. The site is not available. Notwithstanding this, the site has a road frontage but is located in a residential/ edge of rural area and therefore would not be a suitable location for a Big Yellow. Prior approval application for residential use indicates that the site may be pursued for residential.
9	Bnit 1 And 2 And 3 Foundation Park Roxborough Way Maidenhead	RBWM	8.7	1	RBWM Application Search	Offices.	Yes - Business Area	Private	Planning secured for offices and part built out.	Leasehold available. Being pursued for B1 development.	Site not currently available. Site in use/ being developed for offices.	On market - to let only, part of site	Site being developed/ extended for office development. Not suitable for B8. Location on edge of rural area also not suitable for Big Yellow.	Foundation Park marketing website confirms that the site will deliver up to 144,000 SQ FT of new office space. Planning granted in 2019 (ref. 19/01661/DUT) for: Outline application for access, landscaping, layout and scale to be considered at this stage with all other matters to be reserved for the demolition of buildings 1, 2 and 3 and the erection of three class B1 (office) buildings, new decked car park and hub building, the reconfiguration of the car parking and internal road layout and associated works. Reserved matters applications submitted in 2021. Space being marketed to let. Site is not available for B8 development. Notwithstanding this, the location and visibility of the site would also not be suitable for Big Yellow's business requirements.
10	Bray Studios Down Place Water Oakley Windsor SL4 5UG	RBWM	33	2/3	RBWM Application Search	Vacant	No	Private	Undetermined planning application	Being pursued for alternative use. Flood Risk.	Yes - Planning application submitted by Bray Studios Ltd.	No. Planning application submitted by Bray Studios Ltd.	No. Being pursued by others. Flood risk, location and visibility.	Planning application submitted in 2021 (ref. 21/02245/FULL) for: Redevelopment to create new film and TV studios, incorporating demolition, retention of 2no. existing sound stages, rehearsal building, backlot and ancillary offices on a permanent basis, construction of further new production facilities including sound stages, workshops, ancillary offices and other production facilities, use of Waterford House and Steading as ancillary accommodation, parking, landscaping and new vehicular access off Windsor Road. Awaiting Decision. Site is not available for B8 development. Notwithstanding this, the location and visibility of the site would also not be suitable for Big Yellow's business requirements.
11	Units 2-3 Slough Interchange Ind. Estate Warehouse	Slough	Part of a 1.47 acre site (35,893 sqft plus parking available to let)	1	Property Agent. Website/ CoStar	Industrial Building to let	Located in Existing Business Area	Private	None	Only available to let. Development not an option that could be pursued.	No - 35,893 sqft currently on market to let	Yes - leasehold only. Not full site.	No due to quantum of floorspace, tenure, location, visibility.	Located just east of the search area but included due to proximity. Only a lease of an existing 35,893 sqft building is available. This quantum of floorspace would not meet Big Yellow's requirement. Furthermore, the site's location is not suitable as it has poor road visibility being located within an industrial estate.
12	Land Adjacent To 105 Whitby Road, Slough	Slough	1.39	1	Annual Monitoring Report	Under construction - B8 use	Slough Trading Estate - Existing Business Area	Private	Scheme consented 2018	Being pursued for alternative development. Layout of site. Visibility.	Not available. Application pursued by expected future tenant.	No. Being pursued by another party.	No. Being pursued by others. Location and visibility.	Application both outline and reserved matters submitted and consented on behalf of First Choice Building Supplies. Google street view from 2019 shows the frame has been constructed. The site is therefore not available and being pursued by another party. The consented B8 floorspace is 895 sqm/ 9,633 sqft, which is considerably smaller than what Big Yellow require. The site is not on a main road position and has poor visibility to passing traffic and therefore it would not be a site deemed suitable for Big Yellow's business requirements.

	Address of site	Local Authority	Site Area (approx./ acres)	Flood Zone	Source	Vacant or Current Use	Allocated in Local Plan	Ownership	Planning Status	Development Constraints	Being pursued by others	Availability	Suitability	Comments and Conclusion
18	426-430, Bath Road, Slough, SL1 6BB	Slough	0.53	1	Annual Monitoring Report	MOT Testing/ Office	No	Private	Planning Application - P/00442/014	Size. Being pursued for alternative use.	Yes - not on market	No	Yes - also suitable for residential led redevelopment	It is below B's size requirement but has been included due to its proximity to the subject site. Planning permission for demolition of existing buildings and redevelopment to provide up to 60 dwellings. Granted circa 2017. Reserved Matters granted March 2021. Site is not on the market and is not available. It is being pursued for residential development. Hoarding is in place around the site, suggesting development is due to start imminently.
19	396, Bath Road, Slough, SL1 6JA	Slough	0.4	1	Annual Monitoring Report	Retail	Site Allocation SKL1	Private	Pre App PreApp/1180	Size and shape of site	Yes - not on market	No	No due to size and shape	The site is located within the same Site Allocation as the subject site. It is below B's size requirement but has been included due to its proximity to the subject site. Pre application discussions on demolition of existing building and construction of 2no new blocks consisting of 9 storey and 3 storeys. The site is not on the market and is not available. The pre-application discussions indicate that it is being pursued by others, although at the current time the existing built development remains on the site (retail use). Notwithstanding this, the site is too small and its narrow shape would make it unsuitable for a self storage facility.
20	S M C Ford, 134, Bath Road, Slough, SL1 3XW	Slough	1.15	1	Annual Monitoring Report	Car showroom	No	Private	Planning Application P/01160/039	Shape of site	Yes - not on market	No	Yes - proximity to Town Centre is likely to mean demand for alternative uses also	The L shaped site footprint, may result in part of the site not being suitable for self storage use. Planning secured fore Demolition of existing portacabin and 4 bay rear workshop. Construction of new building incorporating a 5 bay workshop, canteen and storage. Annual Monitoring Report 18/ 19 advises that this has not started. The site is not on the market and is not available. The planning permission which proposed to retain the site for car sales use and granted in 2016 indicates that it is being pursued by others/ by the existing land owner or occupier. This permission will have now expired, if not implemented. The site is also still in use and trading as a car showroom/ dealership.
21	Hitcham Place, Taplow Common Road, Burnham, Maidenhead, Berkshire	South Bucks	1.25	1	Property Agent Website/ EGI	Vacant	No	Private	None	Leasehold only available. Green Belt	Not currently - on market	On market - to rent only.	No due to location in Green Belt and also the rural location away from urban area. Being pursued by others.	Open storage land. Majority of land undeveloped. Small temporary buildings on parts of site. Access is via double gates fronting Taplow Common Road. There is water and electricity on site. Site is located in Green Belt and therefore would not be suitable for the quantum of development required by Big Yellow. The rural location also would not suit Big Yellow's business needs. The site is too far from an urban area.
22	Land Adjacent To Taplow Station (Roots Gardens/Southern Motor Homes), Bath Road, Taplow Buckinghamshire, SL6 0NL	South Bucks	2.9	1/ 2	Planning History Search	Undeveloped hardstanding/ Parking	No	Private	2019 Pre-Application Submission - PQ/19/40499/PR EAPP	Tree/ vegetation cover. Long but narrow site. Proximity to railway.	Yes	No	Yes - although further design work would be required to better understand constraint of site depth.	The land is not available. The pre-application engagement indicates that it is being pursued for alternative development. Notwithstanding this, its narrow layout is also unlikely to lend itself to the required layout for a Big Yellow. There is also dense tree/ vegetation cover on and surrounding the site that is likely to impede the site's development potential. The site is not available.
23	Land at Huntercombe Park Huntercombe Lane South Burnham, Buckinghamshire SL6 0PH	South Bucks	7.95	1	Planning History Search	Former Office building	No	Private	Planning Application - Pt/19/2305/FA - Approved	Access	Yes	No	No. Being pursued for other uses. Location not suitable on non main road.	The site does not front on to Bath Road and is also accessed via a fairly narrow and non main road. This access and position would not suit Big Yellow's requirements. Planning permission secured in 2019 for demolition of existing office and construction of 165 residential units on the site. This consent is understood to have been built out (Bellway Homes). Adjacent offices on market to let, these buildings would not be suitable for self storage. The land has been pursued for alternative development and uses and is not available.

	Address of site	Local Authority	Site Area (approx./ acres)	Flood Zone	Source	Vacant or Current Use	Allocated in Local Plan	Ownership	Planning Status	Development Constraints	Being pursued by others	Availability	Suitability	Comments and Conclusion
13	17-31, Elmshott Lane, Slough, Berkshire, SL1 5QS	Slough	1.36	1	Planning Committee Search	Ground floor retail/ with residential above	No	Private	2 x outline applications refused.	Proximity to residential. Narrow width of road access.	Not available. Being pursued by another party for residential development.	No. Being pursued by another party.	No. Being pursued for residential. Location not suitable for Big Yellow's requirements.	P/04670/014- planning refused for: Revised Outline planning application with all matters reserved for the demolition of existing retail/residential buildings. Construction of three storey plus mansard building, over basement to provide 84 flats. Previous outline application also refused. One of the grounds of refusals was the impact on character of area. Site constrained by low rise residential character of area and poor access being location on a non major road. The location would not suit Big Yellow's business needs. Site is not available and is being pursued by another party.
14	Rear Of Atria House, 219, Bath Road, Slough, SL1 4AA	Slough	0.7	1	Planning Committee Search	Car Park	No	Private	Planning for residential granted	Narrow single lane access located to site positioned to the rear of existing commercial unit.	Not available. Being pursued by another party for residential development.	No. Being pursued by another party.	No. Being pursued for residential. Location not suitable for Big Yellow's requirements.	March 2020 planning ref. P/06954/030 granted for: Construction a part three / part four storey detached building to accommodate 37 self contained flats, Associated parking, communal areas, and landscaping. Site is not available and is being pursued by another party. Notwithstanding this, access taken from Bath Road - single lane access road which would not be suitable for Big Yellow's requirements. Tight site at the bottom end of Big Yellow's area criteria.
15	375 Bath Road, Slough	Slough	0.49	1	On Market - CoStar Property Search	Vacant (former HSS Hire)	Site Allocation SKL1	Private - Freehold interest for sale	Pre- Application advice for residential development	Size	Yes - site purchased and no longer on market. Being pursued by another party.	No - site purchased and no longer on market. Being pursued by another party.	Yes - also suitable for residential led redevelopment	The site is currently vacant. The site is located within the same Site Allocation as the subject site. It is below BY's size requirement but has been included due to its proximity to the subject site. The site's position on Bath Road makes it a suitable location for a self storage facility. However, it is also suitable for a more comprehensive residential led redevelopment, as a result of its position in Flood Zone 1. The site is considered by Slough Council in their written pre- application advice (September 2020) to be suitable for a residential led redevelopment. The site is no longer available. In early 2020 Costar confirmed it was Under Offer and it is expected that this will likely be to a residential developer. Land registry now confirms that the transaction has completed.
16	225 Bath Road, Slough	Slough	12.05 (can be subdivided)	1	Part of site no longer on market. Remaining two parcels on Market - CoStar Property Search/ SEGRO website	Vacant - part under construction	No	Private - Segro own and available to lease. One parcel available freehold.	Planning secured - P/00730/091 and P/00730/103	Segro developing	Majority of land no longer on market. Two parcels remain on market.	Majority of land no longer on market. Two parcels remain on market.	No - split over various buildings/ being pursued by others or 0.67 acre parcel is not suitable due to access, layout and proximity to residential.	Owned by SEGRO. The development comprises a large site which offers a design and build scheme from 50,000 sqft to 200,000 sqft. The proposals for the site in the marketing information show 3 industrial buildings across the site. The space would be available to lease, Big Yellow's strong preference is to secure a freehold interest. Given that a leasehold only is available, the build of the scheme is unlikely to be meet the needs of Big Yellow, as the space is targeted at more traditional warehousing/ industrial occupiers. The marketing agent, Cushman and Wakefield's website listed in March 2021 that the site was under offer and the built development on the site is now substantially complete. An online search confirms that planning for Datacentre uses has been secured on the site (P/00730/091 Approved October 2019) and also a vehicle rental premises use (P/00730/103 Approved Dec 2020). The site is being pursued by others, for alternative uses. In addition there are two further parcels of land at the same address that are on the market by SEGRO. The leasehold interest of a 0.48 acre site fronting Bath Road, which would be too small for Big Yellow's requirement. The freehold interest of 0.67 acres of land. It is a long narrow site, backing on to residential and located behind the existing Decathlon and PC World. It is accessed via a narrow single lane access road leading from Bath Road. The site's position, shape and access arrangements would not be suitable for Big Yellow and the site area is also at the bottom end of Big Yellow's search requirement.
17	352 - 372 Bath Road, Slough	Slough	0.84	3	Site Allocation	Vacant	Site Allocation SKL1	Private	None	Flood Zone 3	Yes. Not on market. Understood to have been acquired in 2020/ early 2021.	No	No - not available	The site is vacant and there is no built development on it. Hoarding surrounds the site. The site is located in Flood Zone 3, like the subject site. It is on the north side of Bath Road, opposite the subject site and also within Site Allocation SKL1. The site is therefore very similar in terms of its development potential to the subject site. The site is unavailable and understood to be being pursued by another party.

Registration Date:	N/A	Application No:	PREAPP/13 02
Officer:	Neil Button	Ward:	Central
Applicant:	Green Monarch	Application Type:	Major
Agent:	Ashleigh Cook, Icen Projects, Da Vinci House, 44 Saffron Hill, London EC1N 8FH		
Location:	Queensmere Shopping Centre (including Dukes House and Wellington House), 141, 143, 145 and 165 High Street, and associated land, Slough, SL1 1LN		
Proposal:	Outline application (with all matters reserved) for the demolition of buildings and the phased redevelopment of the site to provide a mixed-use scheme comprising residential; flexible commercial Town Centre Uses floor space (Use Class E and Use Class F), supporting sui-generis town centre uses; car and cycle parking; site wide landscaping and associated servicing and highways works		



PRE-APPLICATION PRESENTATION

Introduction:

SBC has entered into pre-application negotiations with the Applicant [Green Monarch] in connection with the comprehensive redevelopment of the Queensmere Shopping Centre (including Dukes House and Wellington House), 141, 143, 145 and 165 High Street, and associated land. The site comprises a substantial part of Slough Town Centre including a large section of the High Street. The site forms part of Strategic Site Allocation 14 (SSA14) Queensmere and Observatory Shopping Centres designated in the Development Plan Document Site Allocations which forms a part of the Core Strategy 2006-2026.

The Applicant intends to submit an outline planning application (with all matters reserved) for the demolition of buildings and the phased redevelopment of the site to provide a mixed-use scheme comprising residential; flexible commercial Town Centre Uses floor space (Use Class E and Use Class F), supporting sui-generis town centre uses; car and cycle parking; site wide landscaping and associated servicing and highways works.

The Site and Surroundings:

The site comprises the Queensmere Shopping Centre (including Dukes House and Wellington House), 141, 143, 145 and 165 High Street, and associated land. The Site extends to 4.82 ha and through the mix of buildings currently on the Site, it provides a range of retail, leisure and office facilities. There are a row of trees on the High Street and Wellington Street adjacent to the application site.

The site is a shopping centre which was constructed in the early 1970s. It is characterised internally by commercial floorspace with ground floor frontages onto the high street and office floorspace on the floors above.

The Site is demarcated to the north by the A4 (Wellington Street) and to the south by the High Street. On the west it adjoins the Grade II Listed Church of Our Lady Immaculate and St Ethelbert and the St Ethelbert's Presbytery. To the east it adjoins the Observatory Shopping Centre and further afield a range of town centre buildings and retail uses, with residential and industrial areas surrounding the town centre.

The site has limited heritage significance. The Grade II Listed St Ethelbert's Church is located immediately west of the site with the Grade II listed Station to the north. The eastern end of the site is adjacent to the Slough Old Town Area of Special Character.

The High Street (including shopping centre) is located within the designated Town Centre Shopping Centre Area in the Local Plan (2004) and is within the Town Centre Area (Local Plan 2004). The Site is within Flood Zone 1.

Site History:

Ref: P/19264/001 - Environmental Impact Assessment (EIA) request for a Scoping Opinion pursuant to Regulation 15 of the Town and Country Planning (EIA) Regulations 2017 for the comprehensive redevelopment of the Queensmere Shopping Centres to comprise the construction of a phased residential-led, mixed use development including residential,

commercial, business and retail floorspace, car parking, access arrangements and associated landscaping.

Scoping Opinion Issued 22nd September 2021

Ref: P/06684/015 - Partial demolition and internal alterations/extensions to existing shopping centre as part of a part new build/part refurbished mixed used scheme for 11,533 sq m of A1 retail, class A3 - A5 food and drink and class D2 assembly and leisure floor space and 675 residential units. The residential element comprising 346 no. 1 bedroom and 329 no. 2 bedroom being contained within 4 no. Towers of between 15 and 23 storeys plus infilling development on top of the existing shopping centre and a standalone tower of 15 storeys with a viewing galley on top. Reconfiguration of existing access and frontages onto Wellington street and works including, alterations and improvements to the entrances to the shopping centre; provision of amenity space and landscaping; vehicle and cycle parking; refuse and recycling storage; provision of new and/or upgrading existing infrastructure; groundwork's and re-profiling of site levels; ancillary engineering and other operations and plant and machinery.

Resolution to Grant planning permission subject to s106 [Application Withdrawn]

Policy Background (in connection with the Site)

Site Allocation Document (2010) - SSA14 – Queensmere and Observatory Shopping Centres. The Reason for Allocation is:

To establish the principles for the comprehensive redevelopment and/or reconfiguration of the Queensmere and Observatory shopping centres. To ensure that the future development of the shopping centres positively contributes to the wider regeneration proposals for the town centre; particularly the Heart of Slough. To support development proposals that will encourage further retail investment in the town centre.

Applications for planning permission on sites in the Site Allocations DPD will be expected to comply with Slough's Development Plan and other material guidance. The Site Planning Requirements state that Redevelopment and/or reconfiguration proposals should:

- Create an internal pedestrian link between the Queensmere and Observatory shopping centres (already delivered);
- Improve the retail and leisure offer around the Town Square through change of use of key units and improved retail offering
- Link to the Heart of Slough through provision of a western entrance to the shopping centre, and access to residential units above the centre (delivered)
- Create active frontages along the A4 Wellington Street and St Ethelbert's Church frontage
- Remove the service ramp to the Prudential yard in coordination with the Heart of Slough proposals for the area (delivered)
- Improve pedestrian links to the bus and train stations via Wellington Street (part delivered)
- Rationalise multi-storey car parking provision and its links to the centres and Wellington House

- Redevelop the western end of the Queensmere Centre adjacent to St Ethelbert's church, including improved retail units, residential accommodation above the centre and removal of the toilet block;
- Transform the Wellington Street frontage to create an urban boulevard with tree planting, improved north-south route connection to the town centre, active retail frontages and access to residential accommodation above the retail units
- Aim to reduce the negative impacts of construction upon existing businesses and on the quality of life for residents and users of the town centre by appropriate phasing and implementation.

The Centre of Slough Interim Planning Framework (2019) – The Interim Planning Framework (IPF) was prepared to start the process of producing a Master Plan for the Centre of Slough which can help to resolve all of these issues by promoting sustainable growth and investment in the area. The IPF promotes an “activity” led strategy which seeks to maximise the opportunities for everyone to use the centre for a range of cultural, social, leisure and employment activities which are unique to Slough. The IPF comprises a “concept” led strategy which seeks to promote the centre’s potential as location for major employment and housing growth and destination for leisure and cultural activities, all underpinned by a sustainable transport network and world class transport hub.

The Queensmere Site lies within the ‘**Central Area**’ which the IPF requires that the centre of Slough will be an attractive, vibrant hub providing high quality offices, retail, leisure, landmark buildings and cultural opportunities for our diverse communities. By encouraging investment, regeneration, innovation and high standards of design we will have created distinct environments with high quality public realm that creates a sense of place.

The IPF notes that *it is clear that the overall size of the shopping centre will have to be reduced. Whilst there may be the opportunity to introduce many new uses, as part of the activity led strategy, the amount of retail floorspace needed in the centre will be less than it is at present.*

The IPF states that: *The demolition of the precincts and reduction in retail floorspace will allow new uses to be created on the sites including major new residential development, employment uses, replacement retail and new leisure uses. It will also allow the existing car parks to be reconfigured. The principles for development are set out in more detail below.*

The IPF proposes that the redevelopment of the Queensmere (and Observatory Shopping Centres) provide the opportunity to refocus major retail development back onto the High Street which should become the prime frontage for the retail/leisure units.

A key proposal of this Framework is that a new pedestrian route is created from the station to the High Street by extending Brunel Way southwards. This will include creating a super crossing across Wellington Street.

Much of the planning guidance set out in the IPF is replicated and developed further in the CSRF and Emerging Spatial Strategy described below.

The Centre of Slough Regeneration Framework (CSRF) (2020) – Comprises an evidence based study prepared by Urban Initiatives to inform the Council’s Emerging Spatial Strategy. The CSRF envisages that development proposals for the [Queensmere Shopping Centre]

should result in the 'establishment of a mixed-use quarter on this large and prominent site within Slough's urban core'.

The Regeneration Framework suggests some indicative estimates as to what land uses could be provided. It states that there could be a minimum of 1,000 dwellings, 50,000 m² of offices, 10,000 m² of retail and 5,000 m² of leisure including food and beverage and a cinema. *[NB: The Regeneration Framework has been updated to consider the emerging proposals for Queensmere Shopping Centre redevelopment].*

Further detailed development principles are set out in the framework, which elaborate further upon the guiding principles within the Emerging Spatial Strategy (as set out below).

Proposed Spatial Strategy (PSS) (2020) – Concentrating development in the Centre of Slough is at the heart of the Spatial Strategy which will inform the Emerging Local Plan for Slough. The PSS confirms that the Centre of Slough will provide the bulk of housing that will be built in the Borough. The Queensmere Shopping Centre site is proposed to deliver a substantial number of these new homes [in addition to the adjoining Observatory Shopping Centre which will be planned to come forward at a later date].

The PSS notes that **Queensmere shopping centre [and Observatory Shopping Centre]** currently performs many of the traditional town centre functions in Slough, but are outdated and, like many, have lost their attractiveness. As a result, the PSS considers they provide the biggest opportunity for regeneration in the Centre of Slough.

The overall objective within the PSS is to encourage the comprehensive mixed-use redevelopment of the shopping centres which will transform the area into an attractive, vibrant, well connected place that can provide for some of the important shopping, leisure, cultural and business needs" whilst also meeting some of Slough's housing needs. The following planning principles will be applied to the proposed comprehensive mixed use development of the Queensmere Shopping Centre Site. [some principles are also relevant to the Observatory site, which is not currently under consideration and does not form a part of the application site].

The redevelopment [of the Queensmere Site] should provide:

- Comprehensive mixed use scheme which follows the broad principles set out in the Centre of Slough Interim Planning Framework and the Regeneration Framework.
- New **retail and leisure** facilities which will create an attractive and vibrant centre. This will include new **food and beverage outlets and a cinema** which will help to sustain an evening as well as day time economy. This could also include provision for **new cultural facilities**.
- Approx **50,000 m² of offices** could be built along Wellington Street as part of the "workplace led" regeneration strategy.
- A **substantial amount of housing** throughout the redevelopment with a concentration at the eastern end, possibly in a new residential quarter. This should include a range of accommodation including the maximum reasonable provision of **affordable housing**;
- A **high standard of architecture** will be required in order to deliver this dense city centre scale development;
- The tallest buildings should generally be to the north alongside Wellington Street, which should become a new "address street". The height of buildings should step down

towards the High Street in the south where they should generally be a maximum of 6 stories in order to retain its human scale.

- The scale and nature of development at the western end of the sites needs to reflect and enhance the setting of St Ethelbert's church and The Curve;
- A **new pedestrian route** (with active frontages) will be created which links the High Street to the railway station via an improved Brunel Way.
- The whole site will need to be permeable with convenient and attractive links to the rest of the town centre.
- Buildings should be set in a high quality public realm that includes areas of planting and semi-mature tree planting.
- New areas of public open space will have to be provided including a "**civic square**" adjacent to, or part of, the new pedestrian link from the High Street to the railway station.
- Opportunities should be taken to provide landscaped roof gardens on the tops of buildings.
- The views of the surrounding area should be optimised, particularly those of Windsor Castle to the south. A public viewing area could be provided to create an attraction and enhance the appreciation of the area.
- Potential creation of a secondary "neighbourhood" of shops and leisure uses behind the High Street frontage;
- The layout should put pedestrians first.
- Vehicular access to the public car parks should be from Wellington Street;

The Proposal:

The development will include the phased demolition of all buildings and phased redevelopment for up to 350,000 sqm across six Development Zones (DZs), to provide a mixed-use scheme, including:

- Up to 1,600 residential units (Class C3 and up to 20% of which could be Class C2 use), with flexibility in the upper levels of DZ1, DZ 2A&B and DZ4 for either residential or office use, and DZ6 for residential use with sandwich car parking and/or residential use and MSCP, or MSCP for office use only in DZ1/DZ2A/DZ2B/DZ4.
- Up to 550 car parking spaces.
- 0 sqm – 40,000 sqm office space on the upper levels of DZ areas highlighted above.
- 5,500 sqm – 12,000 sqm retail, food and beverage uses (Class E and Class F (Use Class F would not include primary or secondary schools under F1(a))).
- 0 sqm – 1,500 sqm live music venue/cinema (Sui Generis)
- 0 sqm – 2,250 sqm bar/hot food takeaway (Sui Generis)
- Additional flexibility in basement footprint allowance.

The development is planned to be completed in phases from west to east across the site, with demolition and construction starting in 2023. The development is anticipated to be built over 14 years with a completion year of 2036. The Applicant will set out in their presentation to the Planning Committee on 13th October, further specifics of the development, as it currently comprises, at this stage in the pre-application process.

The Application is proposed to be submitted in outline form with all matters reserved. The documents submitted for approval will comprise parameter plans and mandatory design

codes, alongside an Environmental Impact Assessment (EIA). The Scope of the EIA has been agreed with the Local Planning Authority.

The Applicant has carried out a comprehensive Community Stakeholder Engagement exercise throughout the pre-planning application process. This included newsletters (sent to local residents, amenity groups, businesses and other local stakeholders), on-line events and forums, including the provision of an interactive web-site. The proposals have been subject to two Design Review Panel sessions with Design South-East and regular technical meetings have been held with the Local Planning Authority and its team of expert consultants and consultees. The Applicant has carried out briefings with the Lead Members, Ward Councillors and Leader of SBC. A Statement of Community Involvement will be submitted as part of the Planning Application detailing the feedback given during the consultation process to date.

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Developer's briefings to Planning Committee Protocol

Early member engagement in the planning process is encouraged and supported by the NPPF. Enabling a developer to brief and seek the views of elected Members about planning proposals at an early stage (usually pre-application or where this is not possible, very early in the formal application period) is important in ensuring that new development is responsive to and reflects local interests/concerns where possible.

Slough Borough Council proposes to achieve this objective through formal presentations to the Planning Committee in accordance with procedures set out in this Protocol. No decision will be taken at these meetings and if the pre-application submission is followed by a formal planning application, the application will be subject to the normal procedure of a report to a future meeting of the Planning Committee.

1. The purpose of briefings is:

- To enable Members to provide feedback that supports the development of high quality development through the pre- application process, and avoid potential delays at later stages;
- To ensure Members are aware of significant applications prior to them being formally considered by the Planning Committee;
- To make subsequent Planning Committee consideration more informed and effective;
- To ensure issues are identified early in the application process, and improve the quality of applications; and
- To ensure Members are aware when applications raise issues of corporate or strategic importance.

2. What sort of presentations would be covered in the briefings?

Presentations on proposed large-scale developments of more than 50 dwellings, or 5,000m² of commercial or other floorspace or which includes significant social, community, health or education facilities, or where the Planning Manager considers early discussion of the issues would be useful; and

Presentations on other significant applications, such as those critical to the Council's regeneration programmes, significant Council developments, or those requested by the Chair of the Committee or deemed appropriate by the Planning Manager.

3. Frequency and timings of meetings

The presentation will coincide with the monthly Planning Committee meetings.

4. Format of the presentations

- The meeting will be chaired by the Chair of the Planning Committee who will ask Members attending to disclose any relevant interests;
- The Developer will supply all presentation materials including any models, and these will be displayed in the meeting room;
- Officers to introduce the proposal (5 minutes);
- The developer and/or agents will be invited to make a presentation (10 minutes);
- Ward Members will have the opportunity to address the Committee (4 minutes each, subject to the discretion of the Chair);
- Question and answer session: Members of the Planning Committee and Ward Members will be able to ask questions to the Developer and officers (15 minutes) Supplementary questions from Ward members to be at the discretion of the Chair);
- A short note of the meeting summarising Members' comments would be made.

5. Other matters

Members questions will be restricted to points of fact or clarification and must be structured in a way that would not lead to a member being perceived as taking a fixed position on the proposals. Members should ensure that they are not seen to pre-determine or close their mind to any such proposal as otherwise they may then be precluded from participating in determining the application.

MEMBERS' ATTENDANCE RECORD 2021/22
PLANNING COMMITTEE

COUNCILLOR	26/5	23/6	28/7	15/9	13/10	10/11	15/12	19/1	16/2	16/3	13/4
Ajaib	P	P	P	P							
A Cheema	P	P	P	P							
Dar	P	P	P	P							
J Davis	P	P	P	Ap							
R Davis	P	P	P	P							
Gahir	P	P	P	P							
Mann	P	P	P	P							
Muvvala	P	P	Ap	P							
S Parmar	P	P	P	P							

P = Present for whole meeting
Ap = Apologies given

P* = Present for part of meeting
Ab = Absent, no apologies given

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